

Planning Committee Agenda



To: Councillor Toni Letts (Chair)
Councillor Paul Scott (Vice-Chair)
Councillors Muhammad Ali, Sherwan Chowdhury, Chris Clark, Joy Prince,
Jason Perry, Scott Roche, Ian Parker and Gareth Streeter

Reserve Members: Felicity Flynn, Bernadette Khan, Clive Fraser, Leila Ben-Hassel, Helen Redfern, Michael Neal, Badsha Quadir, Jan Buttinger and Andrew Pelling

A meeting of the **Planning Committee** which you are hereby summoned to attend, will be held on **Thursday, 5 December 2019** at **6.30 pm** in **Chamber, Town Hall, Katharine Street, Croydon CR0 1NX**

JACQUELINE HARRIS BAKER
Council Solicitor and Monitoring Officer
London Borough of Croydon
Bernard Weatherill House
8 Mint Walk, Croydon CR0 1EA

Michelle Ossei-Gerning
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www.croydon.gov.uk/meetings
Wednesday, 27 November 2019

Members of the public are welcome to attend this meeting.

If you require any assistance, please contact the person detailed above, on the right-hand side.

To register a request to speak, please either e-mail

Democratic.Services@croydon.gov.uk or phone the number above by 4pm on the Tuesday before the meeting.

THIS MEETING WILL BE WEBCAST LIVE - Click on link to view:

<http://webcasting.croydon.gov.uk>

N.B This meeting will be paperless. The agenda can be accessed online at

www.croydon.gov.uk/meetings

AGENDA – PART A

1. Apologies for absence

To receive any apologies for absence from any members of the Committee.

2. Minutes of Previous Meeting (Pages 7 - 14)

To approve the minutes of the meeting held on Thursday 21 November 2019 as an accurate record.

3. Disclosure of Interest

In accordance with the Council's Code of Conduct and the statutory provisions of the Localism Act, Members and co-opted Members of the Council are reminded that it is a requirement to register disclosable pecuniary interests (DPIs) and gifts and hospitality to the value of which exceeds £50 or multiple gifts and/or instances of hospitality with a cumulative value of £50 or more when received from a single donor within a rolling twelve month period. In addition, Members and co-opted Members are reminded that unless their disclosable pecuniary interest is registered on the register of interests or is the subject of a pending notification to the Monitoring Officer, they are required to disclose those disclosable pecuniary interests at the meeting. This should be done by completing the Disclosure of Interest form and handing it to the Democratic Services representative at the start of the meeting. The Chair will then invite Members to make their disclosure orally at the commencement of Agenda item 3. Completed disclosure forms will be provided to the Monitoring Officer for inclusion on the Register of Members' Interests.

4. Urgent Business (if any)

To receive notice of any business not on the agenda which in the opinion of the Chair, by reason of special circumstances, be considered as a matter of urgency.

5. Development presentations (Pages 15 - 16)

To receive the following presentations on a proposed development:

There are none.

6. Planning applications for decision (Pages 17 - 20)

To consider the accompanying reports by the Director of Planning & Strategic Transport:

6.1 19/02049/FUL Land Rear of 13 to 73 Stafford Road, Duppas Hill Road, Croydon (Pages 21 - 54)

Erection of three buildings comprising 126 residential dwellings, ranging from two to five storeys together with associated access, car parking, cycle parking, landscaping and associated infrastructure works.

Ward: Waddon

Recommendation: Grant permission

6.2 19/02532/FUL 3 Northwood Avenue, Purley, CR8 2ER (Pages 55 - 76)

Demolition of a single family dwelling and erection of one 3-storey block, containing 2 x 3 bedroom, 3 x 2 bedroom and 2 x 1 bedroom units with associated landscaping, 1 parking space, cycle storage and refuse store.

Ward: Purley and Woodcote

Recommendation: Grant permission

6.3 19/03839/FUL 1 The Grange, Firs Road, Kenley CR8 5LH (Pages 77 - 98)

Erection of a new two storey side extension and single storey rear extension and conversion of existing building to provide 9 no. 1 and 2 bedroom flats together with associated access, parking and landscaping.

Ward: Kenley

Recommendation: Grant permission

7. Items referred by Planning Sub-Committee

To consider any item(s) referred by a previous meeting of the Planning Sub-Committee to this Committee for consideration and determination:

There are none.

8. Other planning matters (Pages 99 - 100)

To consider the accompanying report by the Director of Planning & Strategic Transport:

8.1 Weekly Planning Decisions

This report provides a list of cases determined (since the last Planning Committee) providing details of the site and description of development (by Ward), whether the case was determined by officers under delegated powers or by Planning Committee/Sub Committee and the outcome (refusal/approval).

9. Exclusion of the Press & Public

The following motion is to be moved and seconded where it is proposed to exclude the press and public from the remainder of a meeting:

"That, under Section 100A(4) of the Local Government Act, 1972, the press and public be excluded from the meeting for the following items of business on the grounds that it involves the likely disclosure of exempt information falling within those paragraphs indicated in Part 1 of Schedule 12A of the Local Government Act 1972, as amended."

Planning Committee

Meeting of Croydon Council's Planning Committee held on Thursday, 21 November 2019 at 6.00 pm in Council Chamber, Town Hall, Katharine Street, Croydon CR0 1NX

This meeting was Webcast – and is available to view via the Council's Web Site

MINUTES

Present: Councillor Toni Letts (Chair);
Councillor Paul Scott (Vice-Chair);
Councillors Muhammad Ali, Sherwan Chowdhury, Joy Prince, Jason Perry, Ian Parker, Gareth Streeter, Bernadette Khan (In place of Chris Clark) and Michael Neal (In place of Scott Roche)

Also Present: Councillor Margaret Bird and Steve O'Connell

Apologies: Councillor Michael Neal for lateness

PART A

259/19 Minutes of Previous Meeting

RESOLVED that the minutes of the meeting held on Wednesday 6 November 2019 be signed as a correct record.

260/19 Disclosure of Interest

In relation to the application item 19/02678/FUL 64 to 74 Whytecliffe Road North, Purley, CR8 2AR, Councillor Khan declared in the interest of openness and transparency that her son has properties in the area.

In relation to the application item 19/03074/FUL 5 Highland Road, Purley, CR8 2HS, Councillor Khan disclosed that she lived in proximity to the address and so withdrew from consideration of this item.

261/19 Urgent Business (if any)

There was none.

262/19 **Development presentations**

263/19 **19/02842/PRE Croydon College Annexe, College Road, Croydon, CR9 1DX**

Mixed redevelopment of the site to provide circa 90 flats and a 'Cultural and creative industries enterprise centre'.

Ward: Fairfield

Will Edmonds, Partner at Montagu Evans (the Planning Agent), Paul Fender from ECE Architects and Donncha Murphy from Stonegate Homes (the Applicant) attended to give a presentation and respond to Members' questions and issues raised for further consideration prior to submission of a planning application.

The main issues raised at this meeting were as follows:

At 6:37pm Councillor Neal attended the meeting.

Design and massing: Members welcomed the architectural response for a challenging site (including the twisting building form to address the Fairfield public realm, dynamic design of ground floor and artist studios). Questions were raised about the resilience of the proposed materials and the design of the facades to ensure that they are not stark/harsh when viewed from the public realm. The Architect confirmed that they are currently considering using a white concrete for the taller part of the building. There was support for a sculpture facade to create interest in the building. Members discussed the main tower element, querying whether the Applicant had considered whether there was opportunity for additional height, or architectural crown feature similar to the precedent existing buildings shown in the presentation. Members sought clarification about rationale for the 4-story element to the site.

Public Realm: Members welcomed the pedestrian walkway through the site to Barclay Road which would be a significant addition to the area. They questioned whether the undercroft could be increased in size and queried the safety of the undercroft area. Members requested that the undercroft should be well designed, with residential access located to provide high surveillance and well-lit to provide a high quality and safe walkway that would hopefully be well used. It was suggested that a simple walkthrough of the walkway may be helpful to understand how the route works. Members further discussed the finger part of the site, raising concerns that future development above this public realm could be detrimental to the experience and environment of the walkway in terms of safety.

Affordable Housing: Members welcomed the 30% at a 54/46 split in favour of affordable rent, and suggested that the location of the provision and dwelling mix should be identified in the scheme.

Community Space: Introduction of art studios and a café was welcomed but members suggested there was need for flexibility for the café space and continued resilience of the space. There was also a request that the Applicant explore childcare provision within the scheme.

Refuse: Members commented on the refuse arrangements, suggesting that this needs careful consideration and encouraging the Applicant to look at options which would avoid having to use the Barclay Road ramp.

Balconies: There was a request for the balconies to be off the living space. The Architect confirmed that this is the case.

Fairfield Halls: Members sought confirmation that the development is being designed to ensure that it would not prejudice to the operation on the Fairfield Halls. The Architect confirmed that acoustic testing is being undertaken.

The Chair thanked the applicants for their presentation, and looked forward to their application returning to the Committee at a later stage.

264/19 **Planning applications for decision**

The Chair announced that the agenda items would be heard in the following order: 19/03074/FUL 5 Highland Road, Purley, CR8 2HS; 19/03385/FUL 34 Caterham Drive, Coulsdon, CR5 1JF; 19/02678/FUL 64 to 74 Whytecliffe Road North, Purley, CR8 2AR; 19/03410/FUL 22 Purley Knoll, Purley, CR8 3AE and 18/05856/FUL Station Yard, 56 Brigstock Road.

265/19 **19/03074/FUL 5 Highland Road, Purley, CR8 2HS**

Demolition of existing detached house and detached garage and replacement with 9 new apartments in a single block of four / five storeys with parking to the front.

Ward: Kenley

The officers presented details of the planning application and officers responded to questions for clarification.

Mr Christopher Wyatt spoke against the application.

Mr Paul Lewis, the Architect, spoke in support of the application.

Referring Ward Member Councillor Steve O'Connell spoke against the application.

Councillor Perry proposed a motion to **REFUSE** the application on the grounds of over development by dense of its size and massing, it doesn't comply DM 10.1 A and B, causing loss of amenity for adjoining occupants,

impact on highways and insufficient parking with regards to the new London Plan. Councillor Parker seconded the motion.

Councillor Scott proposed a motion to **APPROVE** the application based on the officer's recommendation. Councillor Ali seconded the motion.

The motion to refuse was put forward to the vote and fell with four Members voting in favour and five Members voting against.

The motion to approve was put forward to the vote and carried with five Members voting in favour, four Members voting against.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of 5 Highland Road, Purley, CR8 2HS.

266/19 **19/03385/FUL 34 Caterham Drive, Coulsdon, CR5 1JF**

Demolition of existing dwelling and garage and erection of a three storey building comprising 6 flats (3 x 3 bed, 1 x 2 bed and 2 x 1 bed), 3 parking spaces, communal amenity space and cycle / refuse / recycle storage.

Ward: Old Coulsdon

The officers presented details of the planning application and officers responded to questions for clarification.

Mr Anthony Rumbol spoke against the application.

Mr Tashaan Jain spoke in support of the application.

Referring Ward Member Councillor Margaret Bird spoke against the application.

Councillor Parker proposed a motion to **REFUSE** the application on the grounds of unacceptable flooding risk, over development and out of keeping with the surrounding area. Councillor Perry seconded the motion.

Councillor Scott proposed a motion to **APPROVE** the application based on the officer's recommendations. There was a request for a condition to include that there should be no increase in outflow water from this site that would add to any flooding risk. Councillor Letts seconded the motion.

The motion to refuse was put forward to the vote and fell with four Members voting in favour and six Members voting against.

The motion to approve was put forward to the vote and carried with six Members voting in favour and four Members voting against.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of 34 Caterham Drive, Coulsdon, CR5 1JF.

At 8:20pm the Planning Committee adjourned the meeting.

At 8:20pm Councillor Neal left the meeting.

At 8:29pm the Planning Committee resumed the meeting.

267/19 **19/02678/FUL 64 to 74 Whytecliffe Road North, Purley, CR8 2AR**

Demolition of three pairs of semi-detached houses, erection of a part 3/part 5/part 6 storey building with basement to provide 39 residential units, with associated terraces, disabled car parking spaces, amenity spaces and landscaping.

Ward: Purley Oaks and Riddlesdown

The officers presented details of the planning application and officers responded to questions for clarification.

Mr Michael Dancey spoke against the application.

Mr Kevin Goodwin the Agent, spoke in support of the application.

Councillor Scott proposed a motion for **APPROVAL** of the application based on the officer's recommendation. Councillor Chowdhury seconded the motion.

The motion to approve was put forward to the vote and carried with all nine Members unanimously voting in favour.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of 64 to 74 Whytecliffe Road North, Purley, CR8 2AR.

268/19 **19/03410/FUL 22 Purley Knoll, Purley, CR8 3AE**

Demolition of existing buildings; Erection of 7no. apartments and 2no. dwellings and associated works.

Ward: Purley and Woodcote

The officers presented details of the planning application and there was no questions for clarifications.

Mrs Ella Leonard against the application.

Mr Neal Thompson spoke in support of the application.

Councillor Streeter proposed a motion to **REFUSE** the application on the grounds of over development of site, insufficient parking and damage to the street in the area. Councillor Parker seconded the motion.

Councillor Scott proposed a motion to **APPROVE** the application based on the officer's recommendation. Councillor Ali seconded the motion.

The motion to refuse was put forward to the vote and fell with three Members voting in favour and six Members voting against.

The motion to approve was put forward to the vote and carried with six Members voting in favour and three Members voting against.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of 22 Purley Knoll, Purley, CR8 3AE.

269/19 **18/05856/FUL Station Yard, 56 Brigstock Road**

Demolition of existing builders yard buildings, erection of a block comprising a 6 storey building and a part 7, 8 and 9 storey building comprising 58 residential units (26 x 1 bed, 9 x 2 bed and 23 x 3 bed apartments) and two commercial units (Use Class B1c) along with the provision of associated off-street parking and refuse and cycle storage.

Ward: Bensham Manor

Mr Chris Heather spoke on behalf of the applicant in support of the application.

At 9:49pm the guillotine was lifted for the remainder of this meeting and for the Planning Sub-Committee to convene at the rise of the Planning Committee.

Councillor Scott proposed a motion for **APPROVAL** of the application based on the officer's recommendation. Councillor Ali seconded the motion.

The motion to approve was put forward to the vote and carried with all nine Members unanimously voting in favour.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of Station Yard, 56 Brigstock Road.

270/19 **Items referred by Planning Sub-Committee**

There were none.

271/19 **Other planning matters**

272/19 **Weekly Planning Decisions**

The report was received for information.

The meeting ended at 9.53 pm

Signed:

Date:

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PLANNING COMMITTEE AGENDA

PART 5: Development Presentations

1 INTRODUCTION

- 1.1 This part of the agenda is for the committee to receive presentations on proposed developments, including when they are at the pre-application stage.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 The following information and advice applies to all those reports.

2 ADVICE TO MEMBERS

- 2.1 These proposed developments are being reported to committee to enable members of the committee to view them at an early stage and to comment upon them. They do not constitute applications for planning permission at this stage and any comments made are provisional and subject to full consideration of any subsequent application and the comments received as a result of consultation, publicity and notification.
- 2.2 Members will need to pay careful attention to the probity rules around predisposition, predetermination and bias (set out in the Planning Code of Good Practice Part 5.G of the Council's Constitution). Failure to do so may mean that the Councillor will need to withdraw from the meeting for any subsequent application when it is considered.

3 FURTHER INFORMATION

- 3.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

4 PUBLIC SPEAKING

- 4.1 The Council's constitution only provides for public speaking rights for those applications being reported to Committee in the "Planning Applications for Decision" part of the agenda. Therefore reports on this part of the agenda do not attract public speaking rights.

5 BACKGROUND DOCUMENTS

- 5.1 For further information about the background papers used in the drafting of the reports in part 8 contact Mr P Mills (020 8760 5419).

6 RECOMMENDATION

- 6.1 The Committee is not required to make any decisions with respect to the reports on this part of the agenda. The attached reports are presented as background information.

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PLANNING COMMITTEE AGENDA

PART 6: Planning Applications for Decision

1 INTRODUCTION

- 1.1 In this part of the agenda are reports on planning applications for determination by the Planning Committee.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 Any item that is on the agenda because it has been referred by a Ward Member, GLA Member, MP or Resident Association and none of the person(s)/organisation(s) or their representative(s) have registered their attendance at the Town Hall in accordance with the Council's Constitution (paragraph 3.8 of Part 4K – Planning and Planning Sub-Committee Procedure Rules) the item will be reverted to the Director of Planning and Strategic Transport to deal with under delegated powers and not be considered by the committee.
- 1.4 The following information and advice applies to all reports in this part of the agenda.

2 MATERIAL PLANNING CONSIDERATIONS

- 2.1 The Committee is required to consider planning applications against the development plan and other material planning considerations.
- 2.2 The development plan is:
 - the London Plan (consolidated with Alterations since 2011)
 - the Croydon Local Plan (February 2018)
 - the South London Waste Plan (March 2012)
- 2.3 Decisions must be taken in accordance with section 70(2) of the Town and Country Planning Act 1990 and section 38(6) of the Planning and Compulsory Purchase Act 2004. Section 70(2) of the Town and Country Planning Act 1990 requires the Committee to have regard to the provisions of the Development Plan, so far as material to the application; any local finance considerations, so far as material to the application; and any other material considerations. Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the Committee to make its determination in accordance with the Development Plan unless material planning considerations support a different decision being taken. Whilst third party representations are regarded as material planning considerations (assuming that they raise town planning matters) the primary consideration, irrespective of the number of third party representations received, remains the extent to which planning proposals comply with the Development Plan.
- 2.4 Under Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990, in considering whether to grant planning permission for development which affects listed buildings or their settings, the local planning authority must have special regard to the desirability of preserving the building or its setting or any features of architectural or historic interest it possesses.

- 2.5 Under Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990, in considering whether to grant planning permission for development which affects a conservation area, the local planning authority must pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area.
- 2.6 Under Section 197 of the Town and Country Planning Act 1990, in considering whether to grant planning permission for any development, the local planning authority must ensure, whenever it is appropriate, that adequate provision is made, by the imposition of conditions, for the preservation or planting of trees.
- 2.7 In accordance with Article 31 of the Development Management Procedure Order 2010, Members are invited to agree the recommendations set out in the reports, which have been made on the basis of the analysis of the scheme set out in each report. This analysis has been undertaken on the balance of the policies and any other material considerations set out in the individual reports.
- 2.8 Members are reminded that other areas of legislation covers many aspects of the development process and therefore do not need to be considered as part of determining a planning application. The most common examples are:
- **Building Regulations** deal with structural integrity of buildings, the physical performance of buildings in terms of their consumption of energy, means of escape in case of fire, access to buildings by the Fire Brigade to fight fires etc.
 - Works within the highway are controlled by **Highways Legislation**.
 - **Environmental Health** covers a range of issues including public nuisance, food safety, licensing, pollution control etc.
 - Works on or close to the boundary are covered by the **Party Wall Act**.
 - **Covenants and private rights** over land are enforced separately from planning and should not be taken into account.

3 ROLE OF THE COMMITTEE MEMBERS

- 3.1 The role of Members of the Planning Committee is to make planning decisions on applications presented to the Committee openly, impartially, with sound judgement and for sound planning reasons. In doing so Members should have familiarised themselves with Part 5D of the Council's Constitution 'The Planning Code of Good Practice'. Members should also seek to attend relevant training and briefing sessions organised from time to time for Members.
- 3.2 Members are to exercise their responsibilities with regard to the interests of the London Borough of Croydon as a whole rather than with regard to their particular Ward's interest and issues.

4. THE ROLE OF THE CHAIR

- 4.1 The Chair of the Planning Committee is responsible for the good and orderly running of Planning Committee meetings. The Chair aims to ensure, with the assistance of officers where necessary, that the meeting is run in accordance with the provisions set out in the Council's Constitution and particularly Part 4K of the Constitution 'Planning and Planning Sub-Committee Procedure Rules'. The Chair's most visible responsibility is to ensure that the business of the meeting is conducted effectively and efficiently.
- 4.2 The Chair has discretion in the interests of natural justice to vary the public speaking rules where there is good reason to do so and such reasons will be minuted.

- 4.3 The Chair is also charged with ensuring that the general rules of debate are adhered to (e.g. Members should not speak over each other) and that the debate remains centred on relevant planning considerations.
- 4.4 Notwithstanding the fact that the Chair of the Committee has the above responsibilities, it should be noted that the Chair is a full member of the Committee who is able to take part in debates and vote on items in the same way as any other Member of the Committee. This includes the ability to propose or second motions. It also means that the Chair is entitled to express their views in relation to the applications before the Committee in the same way that other Members of the Committee are so entitled and subject to the same rules set out in the Council's constitution and particularly Planning Code of Good Practice.

5. PROVISION OF INFRASTRUCTURE

- 5.1 In accordance with Policy 8.3 of the London Plan (2011) the Mayor of London has introduced a London wide Community Infrastructure Levy (CIL) to fund Crossrail. Similarly, Croydon CIL is now payable. These would be paid on the commencement of the development. Croydon CIL provides an income stream to the Council to fund the provision of the following types of infrastructure:
- i. Education facilities
 - ii. Health care facilities
 - iii. Projects listed in the Connected Croydon Delivery Programme
 - iv. Public open space
 - v. Public sports and leisure
 - vi. Community facilities
- 5.2 Other forms of necessary infrastructure (as defined in the CIL Regulations) and any mitigation of the development that is necessary will be secured through A S106 agreement. Where these are necessary, it will be explained and specified in the agenda reports.

6. FURTHER INFORMATION

- 6.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

7. PUBLIC SPEAKING

- 7.1 The Council's constitution allows for public speaking on these items in accordance with the rules set out in the constitution and the Chair's discretion.

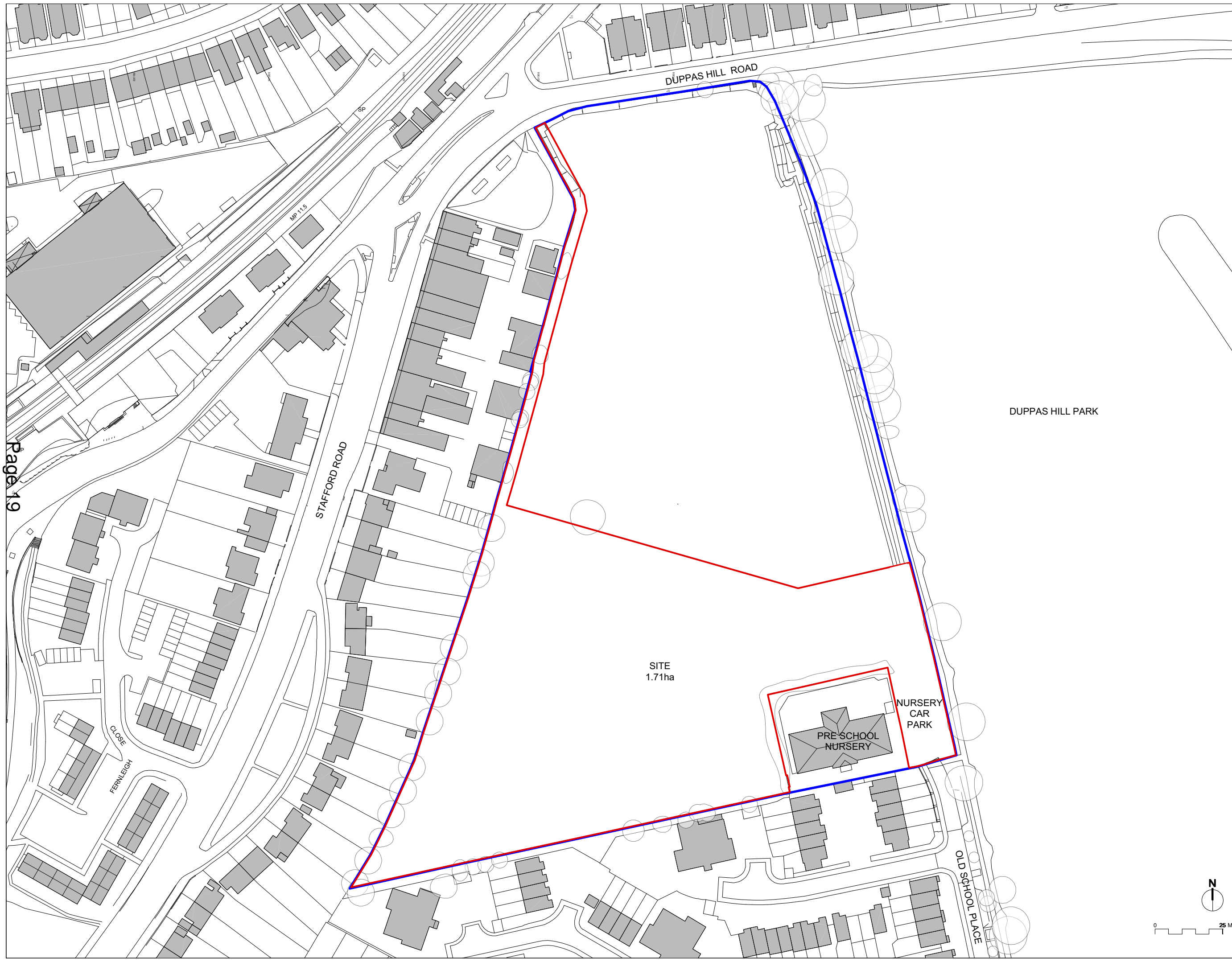
8. BACKGROUND DOCUMENTS

- 8.1 The background papers used in the drafting of the reports in part 6 are generally the planning application file containing the application documents and correspondence associated with the application. Contact Mr P Mills (020 8760 5419) for further information. The submitted planning application documents (but not representations and consultation responses) can be viewed online from the Public Access Planning Register on the Council website at <http://publicaccess.croydon.gov.uk/online-applications>. Click on the link or copy it into an internet browser and go to the page, then enter the planning application number in the search box to access the application.

9. RECOMMENDATION

- 9.1 The Committee to take any decisions recommended in the attached reports.

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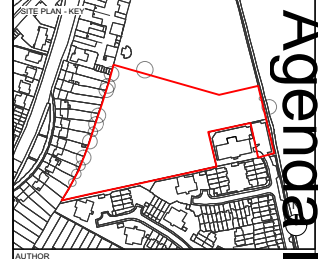
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FOR INFORMATION
 NOTES:
 — SITE OWNERSHIP BOUNDARY
 — SITE BOUNDARY

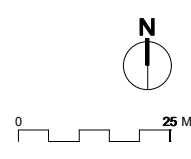
REVISION NOTES

REV	DESCRIPTION	BY	CHK	QA	DATE
P02	FOR PLANNING	BM	SW		30.09.19
P01	FOR PLANNING	LB			24.04.19
P00	FOR PLANNING	BM	LB		10.04.19

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 EXISTING SITE LOCATION PLAN
 PROJECT
 HEATH CLARK, CROYDON
 SCALE
 1:1250
 SIZE
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 DRAWING No
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 REVISION
 P02



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PART 6: Planning Applications for Decision

Item 6.1

1 APPLICATION DETAILS

Ref: 19/02049/FUL
 Location: Land Rear of 13 to 73 Stafford Road, Duppas Hill Road, Croydon
 Ward: Waddon
 Description: Erection of three buildings comprising 126 residential dwellings, ranging from two to five storeys together with associated access, car parking, cycle parking, landscaping and associated infrastructure works.
 Drawing Nos: See Appendix 2
 Applicant: London Strategic Land
 Agent: Savills
 Case Officer: Laura Field

	1 bed 2 person	2 bed 4 person	3 bed 5 person	Total
Market Housing	29	41	18	88
Affordable rent	11	8	8	27
Intermediate	4	4	3	11
All Tenures	44	53	29	126

Number of car parking spaces	Number of cycle spaces
90 plus 24 spaces for the existing nursery	230 long-stay and 8 short-stay cycle parking spaces

1.1 The application is being reported to Planning Committee because the Ward Councillor Robert Canning has made representations in accordance with the Committee Consideration Criteria and requested committee consideration. Moreover, representations above the threshold in the Committee Consideration Criteria have been received.

BACKGROUND

2.1 An earlier version of the proposal was presented to Planning Committee at pre application stage on 12th September 2019. The following comments were made:

Access

- The importance of a pedestrian and cycle route to the western side through the school element was raised
- Pedestrian and cycle access could work on a short term basis until the school is brought forward with a permanent solution
- Cycle routes should tie into proposed new cycle networks on Fiveways and Epsom Road
- Low PTAL for the site - would it be improved with access via Stafford Road?
- Questioned whether the Stafford Road access should be provided now with a vehicular and pedestrian route to Waddon Station
- Preferred option of only providing a gate to public footpath in park seems limited.

- Questioned how the developer would maintain the existing public footpath if it is to be used as a route

Transport

- Questioned the potential transport impact on Old School Place from the residential component
- Difficult and convoluted route into Old School Place
- Pedestrian access from Stafford Road needed
- Vehicular access from Duppas Hill needed
- Has car park stacking been explored?

School

- Questioned the need for the school and when, particularly bearing in mind St Andrews
- Challenged separation of the school and residential sites - difficult to look at in isolation
- More work on school needs to be done

Residential

- Challenged whether more than 127 units could be provided
- Could potentially go higher in built form - southern block in particular
- Questioned whether more affordable housing could come forward
- Questioned the level of social housing and family housing which is an absolute priority
- Concern was raised about the relationship with the nursery and whether the construction period would compromise its viability
- Questioned the refuse collection arrangements and children's playspace
- Results in a cut off and isolated cul-de-sac

2.2 The scheme was presented to PRP on 15th August 2019. Generally, the Panel felt that the scheme should undergo further development to ensure that it connects suitably into the surrounding context, develops a stronger entrance to the site and addresses concerns raised as regards landscape and public realm design. The key recommendations and observations were as follows:

- Excellent landscape design in the centre and the applicant should explore expanding the green space.
- A vital need for direct and safe pedestrian access to Waddon Station.
- Site is land-locked and car-parking pools, amenity spaces and access routes on the site boundaries are poorly overlooked and therefore risk attracting anti-social behaviour. The built form should be amended to address these issues.
- Car dominated and road-widths should be reduced as well as car parking if possible.
- Needs to be a sense of entrance to the scheme and overlooking of it from Old School Place which suffers from several existing dead frontages.
- Design needs to better integrate with the neighbouring Duppas Hill Park and provide more units with direct views of the park.
- Explore including a 5-10 storey development facing the entrance to the site and Duppas Hill Park.
- Generally support the internal layout of the blocks with flats above maisonettes at ground floor level, though some improvements are necessary.

- Recommend the inclusion of the Busy Bees nursery within the scheme and its redevelopment.
- If the nursery cannot be accommodated within the scheme there should be a footway around it to provide direct access to Blocks A and C.
- Need to ensure that the access to the rear of the flatted blocks is adequately secure and overlooked.
- Support the approach to architectural detailing
- Proposed location for the school in the north of the Heath Clarke site is supported.

2.3 The scheme has been amended since the Planning Committee and PRP sessions and has sought to overcome the issues raised above.

3. SUMMARY OF KEY REASONS FOR RECOMMENDATION

3.1 This is an application for the erection of three buildings comprising 126 residential dwellings, ranging from two to five storeys together with associated access, car parking, cycle parking, landscaping and associated infrastructure works.

3.2 The application site is in a current undeveloped green-field state with a site allocation for a secondary school and residential (between 62 to 128 units). The applicant has sought to safeguard land for the school. As such, the principle of a wholly residential use (for this part of the overall site allocation) is acceptable and would contribute towards meeting the Council's housing targets.

3.3 The proposed housing tenure and mix, including the provision of 65% family homes and 30.3% affordable housing (by habitable room) is acceptable and overall would broadly comply with both the policies of the London Plan (2016) and the Croydon Local Plan (2018).

3.4 The design and appearance of the scheme responds positively to its surrounding context, suitably transitioning between two storey housing on Stafford Road and a mix of housing and flats on Old School Place and would thus be acceptable.

3.5 The proposal would have an acceptable impact on the residential amenity of surrounding occupiers, both in terms of daylight and sunlight levels, privacy and outlook for existing surrounding residents.

3.6 The standard of residential accommodation would be acceptable, as all units would meet the Nationally Described Space Standards (NDSS), would have sufficient private amenity space and access to sufficient communal amenity and child play space. All units would have an acceptable level of access to light and outlook.

3.7 The proposed planting and landscaping strategies would create a central high quality communal space and other communal spaces, the detail of which would be conditioned. This is deemed to be acceptable.

3.8 Sufficient disabled car parking and cycle parking has been proposed and the proposal would have an adverse impact upon either the capacity or safety of the local transport network. Pedestrian routes through the site would be subject to conditions and planning obligations.

- 3.9 The proposal complies with the London Plan (2016) energy hierarchy and would provide a carbon offsetting payment to meet the Mayor's requirement for all new homes to be zero carbon.
- 3.10 Suitable planning obligations and conditions have been recommended in order to ensure that the proposed development does not have an adverse impact upon either air quality or the risk of flooding.

4. RECOMMENDATION

4.1 That the Committee resolved to GRANT planning permission subject to:

A. The prior completion of a legal agreement to secure the following planning obligations:

- a) Provision of 30.3% affordable housing (including early and late stage viability review)
- b) Carbon off set payment of £214,920
- c) Air Quality mitigation contribution of £12,600
- d) The future provision of two car club bays (with EVCP) and car club membership
- e) Provision of a Travel Plan
- f) Skills, training and employment strategy and a contribution towards training of £78,600
- g) Section 278 Highway works
- h) A contribution of £8,000 for yellow line waiting restrictions/alternative road markings on roads within the vicinity of the site
- i) Future car parking permit restrictions
- j) CCTV maintenance and monitoring
- k) Safeguarding pedestrian and vehicle routes including for the school
- l) Monitoring fees
- m) Any other planning obligation (s) considered necessary by the Director of Planning and Strategic Transport

4.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the legal agreement indicated above.

4.3 That the Director of Planning and Strategic Transport is delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

- 1) Implemented in accordance with drawings
- 2) Commence within three years of the date of permission
- 3) Submission of a detailed construction methodology including vehicle access and environmental management plan
- 4) Further details of facing materials, balconies, façade and elevational details to be submitted
- 5) Further details of landscaping, highway and pedestrian design and materials, lighting, boundary treatments, child play areas / communal amenity areas and CCTV locations, as well as a maintenance/management plan, to be submitted
- 6) Submission of the pedestrian routes including dimensions, materials, lighting, access and maintenance arrangements

- 7) Site investigation to be carried out and validation report detailing remediation measures to be submitted including a verification report
- 8) No system for inflation of surface water and no piling unless prior agreed
- 9) Submission of Stage 1 written scheme of investigation for archaeology
- 10) Further details of active and passive electric vehicle charging points (EVCP) to be submitted
- 11) Submission of a detailed delivery and servicing plan
- 12) Sustainable development carbon reduction to be met
- 13) Development to achieve 'Secured by Design' accreditation
- 14) Submission of further details of bicycle and bin stores
- 15) Submission of further details on parking, turning, blue badge spaces, visibility splays and sight lines
- 16) Submission of parking management plan to include how the nursery spaces are managed (if these are not used in the future, the spaces will be used for additional landscaping/child's playspace and not be used for further residential parking)
- 17) Submission of a detailed public art strategy
- 18) Submission of a lighting strategy
- 19) Submission of a public art strategy
- 20) Submission of detailed ecological enhancements
- 21) Submission of details of engagement with Thames Water to ensure water network upgrades
- 22) 10% of units to meet Part M4(3), with remaining units to meet Part M4(2)
- 23) Water efficiency targets to be met
- 24) Implemented in accordance with tree protection measures
- 25) Noise from any air handling units, mechanical plant, or any other fixed external mechanical to be at least 10dB below existing background noise levels
- 26) Implementation in accordance with flood risk assessment and drainage strategy
- 27) Implemented in accordance with the recommendations of the noise assessment
- 28) Implemented in accordance with the recommendations of the air quality assessment
- 29) Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Council's 'Construction Code of Practice 2015' and the Mayor of London's 'Control of Dust and Emissions During Construction and Demolition' SPG 2014
- 2) Thames Water informatives
- 3) Section 278 highways informatives
- 4) Subject to legal agreement
- 5) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

4.4 That, if within 6 months the legal agreement has not been completed, the Director of Planning and Strategic Transport is delegated authority to refuse planning permission.

5. SITE LOCATION AND PROPOSAL DETAILS

Site and surroundings

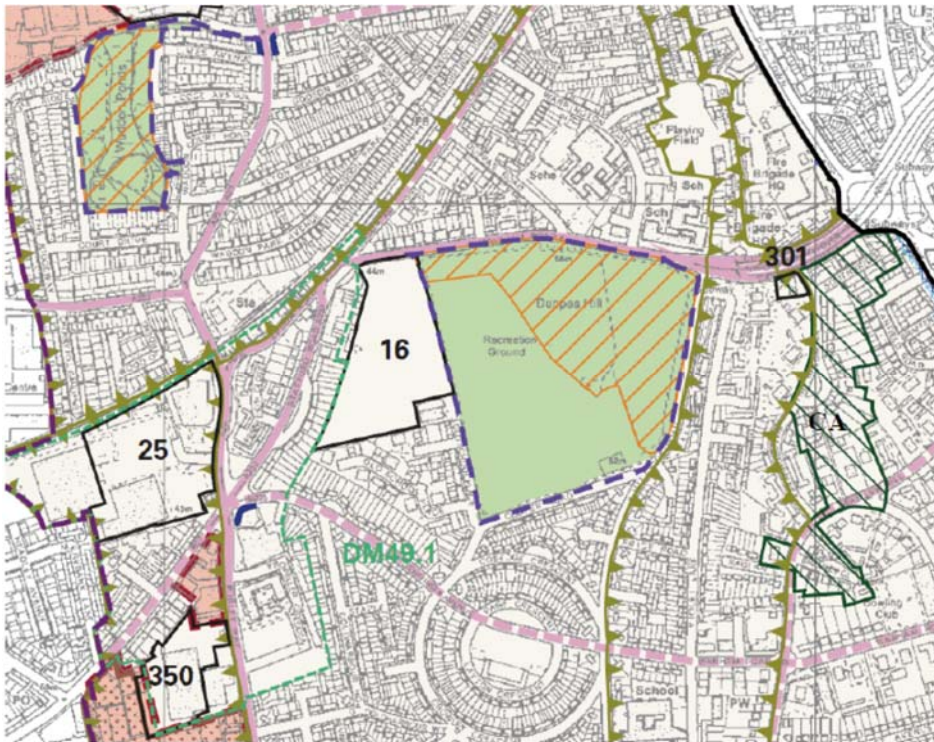


Image 1: Site constraints map (the applicant owns site 16, labelled)



Image 2: The site is outlined in red and the applicants' ownership is outlined in blue to include the wider site. For clarity, London Strategic Land own the wider site; the area in red is the residential element and the area in blue is the land safeguarded as part of the school allocation.

5.1 The application site has an area of 1.71 ha and is currently undeveloped green-field land. The site is bound by existing residential dwellings off Old School Place to the south, existing residential dwellings off Stafford Road to the west and Duppas Hill Park

to the east (which is designated Metropolitan Open Land and a Locally Listed Historic Park and Garden). It is adjacent to 'Waddon', a defined Place in Local Plan (policy DM49.1). Vehicular access is provided by Old School Place to the south, with the existing Busy Bees Nursery located immediately beyond south-east boundary of the application site.

5.2 To the north of the red line site boundary is further undeveloped green-field land which also falls within the ownership of the applicant, but does not form a part of this application site. The application site and the parcel of land to the north of it are together allocated in the Croydon Local Plan for a secondary school and residential development (with a range of number of homes - between 62 to 128) as seen in Table 1 below.

16: Heath Clark, Stafford Road

Place	Postcode	Size of site	Site description	Suburban, Urban or Central location?	Public Transport Accessibility of area	Local character of area	
Waddon	CR0 4NG	3.24ha	Field	Urban	High	Compact houses on relatively small plots; Industrial Estates; Large buildings in an urban setting; Mixed type flats	
Description of option		Justification for option			Anticipated phasing of development	Evidence of deliverability	Number of homes
Secondary School and residential development subject to access from Stafford Road		The site is of a suitable size for a secondary school, is in an area that has a high demand for school places and can make a significant contribution to meeting this demand. The site is also large enough to accommodate new homes as well as a secondary school. Residential development will help to meet the need for new homes in the borough. Access to this site is currently an issue and development is dependent upon the reconfiguration of the Fiveways junction as currently it is not possible to provide access on to Stafford Road or Duppas Hill Road because of the volume of traffic on these roads. The Sustainability Appraisal recommends the loss of open space is mitigated by the development. School buildings and residential development should be located away from areas at risk from surface water ponding.			2021 - 2026	Site has no known developer interest and the Council will need to work with landowner to bring it forward	62 to 128

Table 1: Site allocation from CLP 2018

5.3 The site lies within an Archaeological Priority Area and parts of the site are prone to surface water flooding. The site has a Public Transport Accessibility Level (PTAL) rating of between 1a and 3.

Planning history

5.4 There is no relevant planning application planning history for this site.

Proposal

5.5 The proposed layout takes the form of three blocks; Blocks A, C and D (note there is no Block B – there was in a previous iteration which has been removed). A central public garden between the three blocks at the centre of the site would be provided. The two flatted buildings (Blocks A and D) would be located towards the northern and southern site boundaries. These provide maisonette family homes at the ground and first floors, served by front and rear gardens, with flats above. These blocks would range between two and five storeys in height. Block C would provide terraced dwellings, located towards the western boundary. These would be three storeys in height.

- 5.6 The site would utilise the existing access off Old School Place, with car parking provision as follows:
- 90 spaces for 126 residential units (0.7 spaces per unit), including 13 residential disabled bays and one visitor disabled bay.
 - 24 relocated spaces for the nursery (as per the existing situation).
 - 230 long-stay, 8 short-stay cycles spaces are provided
- 5.7 The 126 homes include a mix of flats, maisonettes and terraced houses. A total of 65% of the dwellings would be suitably sized for families; including 2 bedroom 4 person units.

Type	Number	Percentage
1 bedroom (2 person)	44	35%
2 bedroom (4 person)	53	42%
3 bedroom (5 person)	29	23%
Total	126	100%

Table 2: Proposed Housing Mix

6 CONSULTATION RESPONSE

6.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.2 The following were consulted regarding the application:

Environment Agency (Statutory Consultee)

6.3 No objection subject to pre-commencement conditions in relation to contamination. [OFFICER COMMENT: Conditions are recommended]

Lead Local Flood Authority (Statutory Consultee)

6.4 No objection subject to compliance with the Flood Risk Assessment [OFFICER COMMENT: Conditions are recommended]

Historic England (Archaeology) (Statutory Consultee)

6.5 No objection subject to pre-commencement conditions in relation to archaeology. [OFFICER COMMENT: Conditions are recommended]

Thames Water

6.6 Thames Water made the following comments:

- With regard to the waste water network and waste water process infrastructure capacity, Thames Water have no objection.
- An informative stating the necessity for the applicant to obtain a Groundwater Risk Management Permit and information on underground assets should be included on the decision notice in the event planning permission is granted. [OFFICER COMMENT: Informative is recommended]
- Thames Water has identified an inability of the existing water network infrastructure to accommodate the needs of this development and has requested a condition that

restricts the first occupation of the proposed development until such time that the necessary works have been undertaken [OFFICER COMMENT: The applicant has subsequently provided evidence that they have been in discussions with Thames Water to ensure that the necessary upgrades are made in time for first occupation and have accepted the condition].

Crime Prevention Officer

- 6.7 The proposed development is suitable to achieve ‘Secured by Design’ accreditation, and as such a condition should be attached to ensure that the development follows the principles and physical security requirements (including CCTV and lighting) of ‘Secured by Design’ Further details are also required on the access route to Duppas Hill Road. [OFFICER COMMENT: Conditions are recommended].

Transport for London (TfL)

- 6.8 The proposals are compliant with the transport policies of the draft London Plan. TfL therefore has no further comment subject to conditions on cycle parking, blue badge space and Electric Vehicle Charing Points and Construction Logistic Plan [OFFICER COMMENT: Condition are recommended).

7 LOCAL REPRESENTATION

- 7.1 A total of 291 neighbouring properties were notified about the application and invited to comment and the application was also advertised by site notice and in the local press. The number of representations received from neighbours, local groups etc. in response to notification and publicity of the application were as follows:

No of individual responses: 22 Objecting: 22 Supporting: 0

- 7.2 The following issues were raised in representations that are material to the determination of the application, and they are addressed in substance in the next section of this report:

Summary of Objectors Concerns	Officer’s Response
Impact on parking and safety of local highway network	The parking provision and impact on the local highway network is considered appropriate
The proposal is an overdevelopment of the site and is not in keeping with its surroundings	The development is considered to be of an appropriate scale and form, and is a high quality design.
Overlooking and loss of privacy	The development would not cause an unacceptable loss of neighbouring privacy.
Loss of daylight and sunlight to surrounding properties	The development would not cause unacceptable loss of light and daylight to neighbouring properties.
The footpath is unacceptable linking to	The footpath would create a link between the development and Waddon Station. A condition is also recommended in regards to detail of the footpath

a busy road and business road	
Detrimental impact on trees and more replacement trees required	There is a replacement planting and landscaping scheme. The trees which are to be removed are of low quality.
Increase in noise and disturbance and litter and pollution	It is not considered the proposal would generate significant levels of noise disturbance, pollution and litter given the residential nature of the development and its location.
Noise, disruption and pollution impacts during construction	A draft construction logistics plan has been provided, the final plan is recommended to be secured via condition.
Insufficient capacity of local infrastructure and transport	The proposed development would be CIL liable and would thus contribute towards such infrastructure.
Site should be a playground for new school	The site allocation is both for residential and a school.
Reduction in visual amenity and open space and park should be maintained	The site allocation is for residential and as well as a school. Duppas Hill Park is maintained as part of the proposals.
Detrimental impact on park and local environment	The proposals include landscaping and links to Duppas Hill Park. The proposal is considered to be acceptable in this regard.

7.3 The following issues were raised in representations, but they are not material to the determination of the application:

7.4 The proposed development will have an adverse impact on property prices, private road covenants and views [*OFFICER COMMENT: Property values, covenants and views are not a material planning consideration.*]

7.5 The Ward Councillor for Waddon (Cllr Robert Canning) raised the following objections:

- Difficult and dangerous highway access to the site - both for construction traffic and for vehicles once the flats were built. Accessing the site via Old School Place could put the lives of young children at the nursery at risk. It would also cause congestion problems, disturbance to residents and create new parking pressures in Old School Place and other local roads.

8 RELEVANT PLANNING POLICIES AND GUIDANCE

8.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Croydon Local Plan (2018), Mayor's London Plan (2016) and the South London Waste Plan (2012). Details of the relevant policies and guidance notes are attached in Appendix 1.

National Guidance

8.2 Government guidance is contained in the National Planning Policy Framework (NPPF), revised in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Delivering a sufficient supply of homes;
- Promoting healthy and safe communities;
- Promoting sustainable transport;
- Making effective use of land;
- Achieving well-designed places

Development Plan

8.3 The Development Plan comprises the London Plan 2016, the Croydon Local Plan 2018, and the South London Waste Plan 2012. The relevant policies to this proposed development have been listed in Appendix 1 of this report.

8.4 A replacement Draft London Plan has been subject to public consultation and Examination in Public commenced in January 2019. The current 2016 London Plan is still the adopted Development Plan, and although the Draft London Plan is a material consideration in planning decisions at present it carries limited weight.

9 MATERIAL PLANNING CONSIDERATIONS

9.1 The main planning issues raised by the application that the committee must consider are:

1. Principle of Development
2. Housing (mix and affordable)
3. Townscape and Visual Impact
4. Impact upon Neighbours
5. Housing Quality for Future Occupiers
6. Transport
7. Environmental Impact, Sustainability and Flooding
8. Other Planning Issues

Principle of Development

School Allocation

9.2 To progress this planning application, officers have required the applicant to demonstrate that the area of the site it proposes to retain in an undeveloped state would be an appropriate and feasible location for a future secondary school, in line with the site allocation requirements. Officers have been clear that the school part of the allocation takes priority and the residential scheme must not close off options for a school to come forward in the future.

9.3 Officers have worked closely with education colleagues to ensure sufficient space is safeguarded for the required size of secondary school. A key aspect of the school feasibility work has been showing that vehicular access to a future school would not

be compromised by the fixing of a residential development, particularly bearing in mind the allocation specifically refers to access from Stafford Road. The current proposal does not reduce the potential access for a future school from Old School Place, Stafford Road or Duppas Hill. Traffic modelling has been undertaken in consultation with TfL which has concluded that it could well be possible to provide an access for the school directly off Duppas Hill Road. Further modelling and transport assessments would need to be submitted with any future school planning application. Safeguarding the future access to the school would form a planning obligation incorporated as part of a future S.106 Agreement. Whilst officer's preference has always been for both sites to come forward together as a hybrid application, the applicant has provided a Safeguarding the Future School Site Feasibility Study and officers are satisfied the school has been adequately safeguarded in line with the site allocation, subject to conditions and planning obligations.

- 9.4 Officers have noted that St Andrews School is closing. However, it has been assumed for education modelling (between now and 2039) that some secondary provision will be re-provided on site in the future. Should neither of the two sites (St Andrews and Heath Clark (the application site)) come forward, there would be an undersupply of secondary provision in the future.

Housing

- 9.5 The site allocation indicates a range of homes between 62 and 128. The proposal is for 126 units and therefore at the upper end of the range. It would therefore see the provision of additional dwellings within the parameters of the site allocation.
- 9.6 Officers have challenged the applicant to deliver more units on the site, thereby increasing the density. They have stated the scheme has been designed to be consistent with the site allocation and an increase in height (for example) would compromise the quality of the development particularly in relation to provision of landscaping across the site, additional parking requirements and sunlight and outlook. Given the scheme would deliver close to the upper limit of the allocation, a grounds for refusal (on grounds of under-utilisation of the site) would not represent a sustainable reason for refusal.
- 9.7 The scheme safeguards land for the school and provides homes within the site allocation indicative range and is acceptable, subject to consideration of the following issues.

Housing Policy

Mix

- 9.8 As the site sits within a 'urban setting' with a PTAL of between 1a and 3, major proposals in this location are required to provide a minimum of 60% three-bed units. As outlined in Table 2 above, the proposal provides a total of 23% three bedroom properties. Policy DM1 allows for 2 bedroom 4 person homes to be part of 60% within three years of the adoption of the plan. Overall, 65% of units would be family sized units when including the 2 bedroom 4 person flats which would exceed the current minimum requirement. This is fully supported.

9.9 Moreover, there is a mix of unit sizes through the affordable housing element of the scheme, which is welcomed. The remaining mix of units proposed throughout the scheme would be acceptable and would provide a broad range of unit sizes.

Affordable Housing

9.10 Policy requires the Council to seek a minimum of 30% affordable housing, but to negotiate to achieve up to 50% affordable housing (subject to viability) and seek a 60:40 split between affordable rented homes and intermediate homes.

9.11 The proposed development would provide 30.3 % affordable housing by habitable room, which would amount to 38 units. The tenure splits 71% affordable rented against 29% intermediate housing by habitable room. London Affordable Rent units (a low cost rented product supported by the Mayor of London based on social rent levels which are considerably lower than typical affordable rent levels) form the affordable rented provision. Shared Ownership units form the intermediate provision.

9.12 Whilst the proposed tenure split differs from that set out in policy SP2.4 (60:40), there is a greater proportion of affordable rent being proposed (at London Affordable Rent levels). The applicant's viability assessment has been independently reviewed on behalf of the Council, with this exercise determining that the offer is the maximum reasonable that the scheme can deliver. An early and late viability review would be secured as part of the S.106 Agreement.

Townscape and Visual Impact

Scale and Mass

9.13 The townscape context of the site varies in terms of its urban grain; Stafford Road consists mainly of 2 storey houses and further to the south west is a mixture of houses and flatted blocks of between 2 and 5 storeys. The proposed height of 2 to 5 storeys would follow this context. Whilst officers are of the view the scheme presents a "safe" option in terms of the scale of the buildings, as stated above in paragraph 9.5, it would sit at the upper end of the residential component of the allocation which would be appropriate and in accordance with the development plan. The applicant has stated that the density would be appropriate given the site allocation, the impact on parking, quality of accommodation and the layout. Overall, it is acknowledged the scheme would sit within the range set in the site allocation and the density range in the London Plan (acknowledging this is being removed in the New London Plan and is not to be used mechanistically).



Image 3: Site massing

Site Layout, Amenity and Internal Layout

9.14 Officers are supportive of locating the 3 blocks around a central public space, which would be well overlooked and create a social and active heart to the scheme.



Image 4: Site layout

9.15 The two apartment buildings (blocks A and D) would be located to the north and south. These provide duplex/maisonette family homes at the ground and first floors, with front and rear gardens providing active frontages. Apartments would sit above, accessed by

communal entrances, which would be suitably clear and legible. Block C would provide residential terrace, located alongside with the western boundary and would act as a buffer to the existing semi-detached properties fronting onto Stafford Road.

- 9.16 The site arrangement seeks to take advantage of Duppas Hill Recreation Ground, creating some views towards the park through the positioning of habitable rooms on the eastern flank elevations of the two flatted blocks. The apartment layouts have been developed to omit single-aspect, north-facing units which is supported.
- 9.17 Officers have been clear that pedestrian and cycle routes through the site and linking, in particular to Waddon station are critical to the schemes success. The applicant has now provided a pedestrian and cyclist link along the western boundary of the site - see Image 5 below. This would be lit and accessed controlled for residents only. Furthermore, the applicant has safeguarded the north-west corner for potential future routes through to Stafford Road.
- 9.18 Once the school site is brought forward the access option must be integrated and explored to allow the possibility for further permeability onto Stafford Road, in particular.



Image 5: Site access plan

- 9.19 Officers have raised concerns the pedestrian and cyclist experience entering the site from Old School Place. The applicants has sought to address this by improved passive surveillance, additional pedestrian pathways and enlarged the routes to parking. The

applicants have also committed to CCTV, including maintenance and monitoring. This includes routes through to the park. This is secured through conditions and the legal agreement.

9.20 During the course of the application, the applicant have also removed 11 parking bays and reduced road widths.

Elevational Treatment

9.21 The architectural language chosen for the proposal seeks to integrate with its immediate context whilst developing its own identity, in terms of scale and materiality. The images below illustrate the architectural treatment. The scheme employs a variety of materials, to create design interest which are applied in a contemporary fashion to create rhythm, design interest and break up the mass. The general approach is supported. Given the predominance of brick and masonry-finished buildings in the immediate context of the site, a selection of clay bricks and masonry concrete blocks are proposed which is supported.

9.21 The façade treatment concept is to provide high-quality and long-lasting façades whilst enabling a unique identity for each of the blocks. A different colour of brickwork is proposed for each of the apartment blocks and for the row of terrace homes.

9.22 Within the flatted blocks, the ground floor and parts of the first floor are to be treated in a single unifying masonry blockwork treatment. The architectural appearance of the buildings would be defined by this unifying ‘plinth’ which would provide a single material treatment across the entire development. It is this feature that would tie all of the buildings together, while an individual colour associated with each apartment block should establish a strong individual identity to each block and communal entrance.

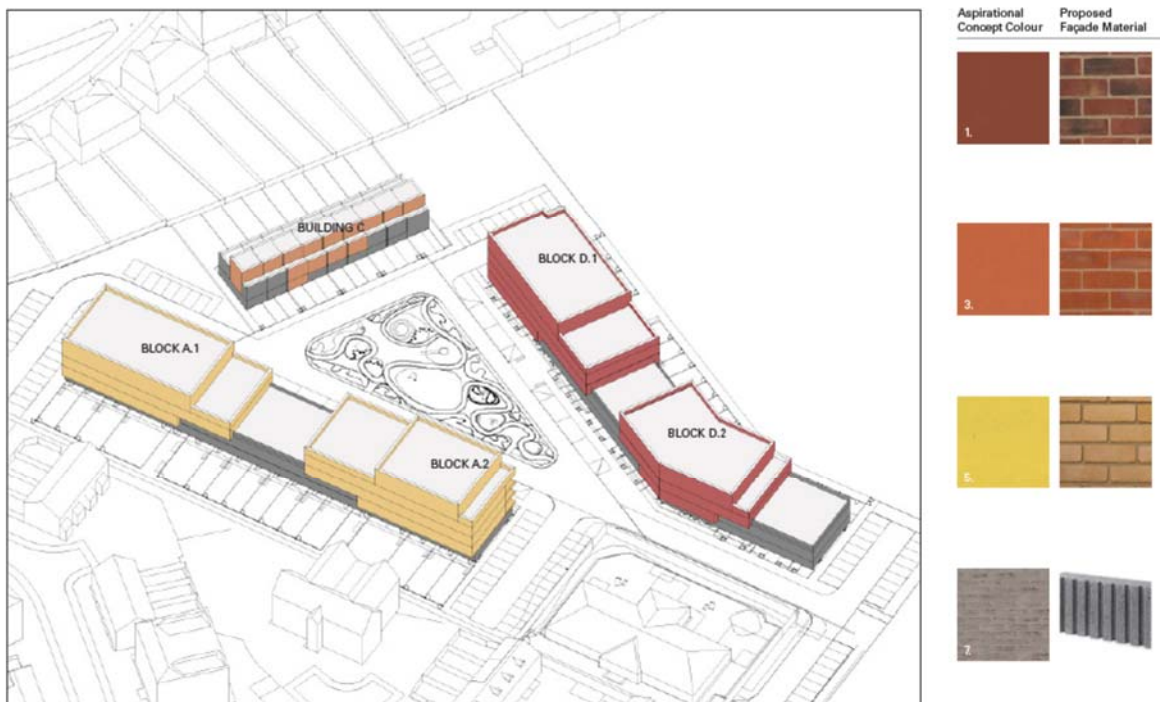


Image 6: Elevational materials

- 9.23 Across the development, secondary façade elements would be consistent across the various buildings where appropriate; for example, typical window types would be common across the multiple buildings with balcony detailing used in all instances. The architectural approach utilising high-quality standardised components that relies on the colour of these elements to provide differentiation and individual identity is supported.
- 9.24 Balconies have been designed as steel-framed and would have a powder-coated finish to match the block colour – adding visual interest to the facades. In limited areas, it is proposed to add a feature brickwork relief, to add further interest and diversity to the overall composition and to help break down the scale of the building. The feature brickwork is proposed as alternating courses of inset brickwork, providing a horizontal banding of relief.





Image 7: Visuals of the scheme

- 9.25 The varying heights would also add interest and maximise the outlook to the park and allow for further dual aspect units. The roof treatment across all buildings is proposed as 'brown' bio-diverse roofs with photovoltaic solar arrays. Where apartments are located adjacent to roof areas, private roof terraces are proposed. Overall, the materials palette is supported, subject to conditions covering the finer details of the elevational treatment, external facing materials (including samples), balconies, refuse storage, lighting and public art.
- 9.26 The elevational treatment to Block A includes yellow bricks over the plinth. Balconies are mainly to the south to maximise the daylight to private amenity spaces. The northern elevation is articulated by the active frontage to the maisonette homes and ground floor flats. The upper levels of the apartment buildings are defined by long curtain wall glazing to the stair cores, which would give legibility to the communal entrances at ground levels. The gable ends of the blocks include recessed balconies at the corners and windows providing passive surveillance to landscape and parking spaces.
- 9.27 The terrace houses of Block C are located between the flatted blocks and at three storeys, would be lower in height and proportion to the adjacent apartment buildings. The elevational treatment would be the same materials in a slightly different way, to tie the architectural response together whilst also seeking to give each terrace house an identity of its own. Orange tone bricks are proposed for the plinth; while the same window proportions would be used as the flatted blocks.
- 9.28 The elevational treatment to Block D would include red bricks over the predominately two storey 'plinth'. Balconies would be typically arranged to the face south and maximise natural daylight to the amenity space, providing a contrast to Block A elevational treatment opposite. In the same way as Block A, there would be curtain wall glazing and recessed balconies and windows to provide passive surveillance. The building height would also increase further from Duppas Hill Park to avoid the sense

of enclosure and maintain views. All these elements would add visual interest to all three blocks and should help embed the development into its local context and help reinforce local distinctiveness.



Image 8: Typical elevation of Block D showing the communal entrance and the treatment of the two storey maisonette homes

Impact upon neighbours

9.29 The siting and massing of the scheme has been designed to respect the adjoining occupiers. The proposals are well separated from the nearest neighbours particularly those residing in properties fronting onto Stafford Road and Old School Place. The surrounding properties that have the most potential to be affected by the proposals are indicated by Image 9.



Image 9: Surrounding properties in relation to the application site

Daylight and Sunlight Impacts

9.30 The applicant has submitted a sunlight and daylight study that tests the scheme against guidance contained with BRE's 'Site Layout Planning for Daylight and Sunlight - A Guide to Good Practice' End Edition, 2011'. See Appendix 3 for BRE sunlight and daylight definitions. The assessment measures the impact of the development on the following properties; Catherine Lodge, 47-69 Stafford Road (odds only), 51-66 Old School Place, 67-70 and 71-74 Old School Place, 140-153 Old School Place and 154-158 Old School Place.

140-153 Old School Place

9.31 The main impact of the development would be 140-153 Old School Place. This building is orientated directly towards Block A, a 4-storey residential block towards the southern boundary. Given these properties overlook an open site in their existing condition, it is inevitable there will be a degree of change from the current daylight levels.

9.32 In terms of daylight, the Vertical Sky Component (VSC) assessment demonstrates that 25 of the 34 windows tested will meet the BRE targets achieving at least 0.8 times their existing levels. In respect of 9 the windows falling below the targets, 4 are living rooms served by additional windows that experience no material change (achieving 0.6 and 0.7 times their existing levels). Where rooms are served by more than 1 window, the BRE guidelines suggest the mean target can be taken. The mean value to these spaces would either meet or exceed the BRE target and therefore would not be materially affected and would be compliant with the BRE criteria. The remaining 4 windows serve bedrooms which are considered to require less daylight under the BRE guide such that these localised effects are considered to be acceptable. 2 of these windows would achieve 0.6 times their existing levels and 2 windows would achieve 0.7 times their existing value.

9.33 Further assessments of daylight distribution confirm daylight levels will not be materially affected as all main living areas achieve at least 0.8 times their existing

value. The only affected spaces are bedrooms which are less sensitive (falling below BRE guidelines by 2 windows achieving 0.5 times their existing levels, 1 window achieving 0.6 and 1 window achieving 0.4 times their existing levels). As such that the effects are therefore considered to have a minor adverse impact and acceptable under the BRE guidelines.

- 9.34 In regards to sunlight, there are 3 main living rooms with windows facing within 90 degrees south and these have been considered for sunlight effects. The results of the APSH assessments show all main living rooms would meet the BRE targets and is compliant for sun-lighting under the BRE criteria.
- 9.35 Whilst the proposed development would result in some daylight impacts, in the vast majority of instances where impacts beyond BRE guidelines occur, these are only minor in nature and where these impacts occur, good levels of daylight and sunlight are generally still maintained. It should be noted that daylight and sunlight impacts for surrounding properties beyond BRE guidelines are inevitable in a situation such as this where the existing baseline condition is a clear site. As such, the daylight and sunlight implications of the proposed development for this immediate neighbour, balancing the benefits of the scheme, would be acceptable.

Other Surrounding Properties

- 9.36 Properties on Stafford Road and other properties on Old School Place are located sufficient distances back from the site boundaries – and with the scale of development proposed and existing orientation, the scheme would satisfy BRE criteria and guidelines.
- 9.37 The existing nursery (Busy Bees-164 Old School Place) was not tested within the applicant's daylight and daylight study due this not being a residential use. The nursery have also provided a letter which states that they have been in continual dialogue with the applicant and the proposal would not impact on the operation of the nursery.

Overlooking, Outlook and Privacy

- 9.38 In respect of surrounding properties, the majority of the properties fronting onto Old School Place sit at an angle to the application which helps when seeking to manage levels of overlooking. 140-153 Old School Place is located immediately to the south and a minimum set-back of 18 metres from the existing buildings would be maintained and existing occupants would continue to enjoy acceptable levels of outlook and privacy. There would be some overlooking – but at an acceptable level.
- 9.39 Existing properties on Stafford Road are situated over 40 metres away from the rear of the terrace houses on Block C. Given this relationship, the development would not unduly overlook these properties or unacceptably impact upon the levels of outlook and privacy afforded to these residents.

Other Impacts

- 9.40 Given that the building is solely residential, there are no concerns that the proposed building would cause noise and disturbance levels that would be incompatible with the surrounding existing uses. Whilst concerns have been raised regarding the impact of construction; such impacts would only be temporary and should only be afforded limited weight. In order to ensure that any such impacts are minimised as far as

reasonable possible, a condition requiring the submission of a detailed Construction Management Plan/Construction Logistics is recommended.

Housing Quality for Future Occupiers

Housing Standards

- 9.41 All of the proposed units would comply with the NDSS and all would feature generous external amenity spaces (in the form of balconies/terraces) which would provide a minimum depth of 1.5 metres (in accordance with the Mayor of London's Housing SPG) and meet the minimum quantum's stipulated by policy DM10.4 of the CLP.
- 9.42 The three buildings would be orientated around the perimeter of the site, parallel to the north, west and south boundaries. To provide active frontages, the buildings would be arranged to form a central public garden with front doors facing onto this space. The ground floor would be arranged to maximise active frontages and the ground floor accommodation on Block A and D would be typically maisonettes, family homes or single level dual aspect units. With the maisonettes, the living rooms would face south.
- 9.43 Of the 126 units proposed, a very high proportion of these would be dual aspect (representing 69% of the proposed units). 23% of the units would be either terrace houses or maisonettes with front and rear gardens. None of the units would be north facing single aspect units.
- 9.44 A daylight and sunlight assessment demonstrates the levels of daylight and sunlight anticipated for the proposed development. The analysis shows that 291 (97%) of the 298 assessed would fully meet the BRE targets for Average Daylight Factor. Of the 6 rooms which would fall short of the targets, 2 are bedrooms which are regarded as less important for daylight under BRE guidelines. These rooms would fail the ADF, being at 0.8 which is considered to be a minor adverse impact. The remaining failures relate to living/kitchen/dining rooms (with ADF of 0.8, 1.0, 1.1 and 1.3). These are set back beneath balconies and whilst external balconies can maximise the overall quality of units, they can result in some lower internal daylight levels. This trade-off is recognised in BRE guidance. These deviations are considered to be minor given the high overall level of compliance.
- 9.45 The sunlight was also assessed for amenity spaces. Whilst the majority of spaces would meet the guidelines, the private amenity space to the north of Block D would fall below the targets (the space will fail to receive at least 2 hours of sun on March 21st – in view of the northerly aspect to these units. That said, the assessment indicates that on 21st June, sunlight levels to these gardens would be over 60% sunlight in the summer months, when the use of the space will be at its highest. Therefore, the limited level of sunlight during spring and autumn months would be a minor adverse impact on future occupiers – but acceptable all the same. The occupiers would also have access to the high-quality central communal garden space.
- 9.46 Careful consideration of the internal layout has been given in order to ensure that future occupiers would be afforded good levels of outlook and privacy, with limited opportunities for overlooking within the development given the angles of each of the blocks. As such it is considered that future occupiers of the proposed development will be afforded a good level of amenity.

9.47 The applicants have submitted a noise assessment to establish existing noise and vibration levels and to identify where and what mitigation measures are necessary. It is proposed that double glazing and mechanical-extract ventilation be provided, to ensure future occupiers are not adversely impacted by noise and vibration levels; these measures, as recommended within the submitted report, are to be controlled through the use of a planning condition.

Communal Amenity and Child Play Space

9.48 In accordance with Policy DM10.5, communal amenity space would be accommodated within the central spaces and there would be a community garden and other landscaped areas throughout the development

9.49 The proposed development is required to provide a minimum of 486 square metres of child play space based on the child yield. In accordance with DM10.4 there would be a total of 570 square metres of playable areas. The central space would feature a range of play spaces providing stimulating activity for children of all ages including natural play features. This area would be inclusive and feature areas of seating as well as soft landscaping. Other incidental and natural play components would provide 'play-on-the-way' and these would also be integrated within the landscape, amongst the planting. Stepping stones, boulders and balancing logs would create 'trails' through the gardens.

9.50 Officers are satisfied that both the proposed communal amenity and child play space would be of a high standard and a feature of the scheme and a condition requiring the submission of the final detailed specification is proposed. All space would be available for all future occupiers.

Accessible Housing

9.51 There are a number of residential entrances to each of the proposed buildings. In general, the apartment units would be accessed from a communal entrance accessed from the roadway, in front of the communal garden. Each building would have a lobby to accommodate post boxes – leading through to stairs and lifts to the upper levels. The townhouses of Block C and the maisonettes on Block A and D would have their own individual front door access, adjacent to the roadway in front of the communal garden. 10% of the proposed units comply with Part M4(3) (Wheelchair User Dwellings) with the remaining units all being designed to comply with Part M4(2) (Accessible and Adaptable Dwellings). The proposed M4(3) units would be distributed across the various tenures. All 13 wheelchair accessible homes would be provided with a parking space in close proximity to their home with an additional accessible space for visitors

9.52 There are up to 8 units per core and therefore the development would be in accordance with the Mayor of London's Housing SPG.

Trees, Landscaping, Biodiversity and Sustainability

Trees

9.53 The existing site is grassland currently being grazed by horses. The applicant proposes to remove three individual trees and a part of a group of trees. These are Category C trees and the loss is accepted by the Council's Tree Officer. Tree protection measures

will be put in place to ensure the retained trees are not damaged throughout the construction phase. The applicant also intends to plant a substantial number of new trees, varying in species type and maturity, which is supported. Consequently, there would be an uplift in trees across the site as a result of the scheme coming forward.

Landscaping

- 9.54 The proposed landscaping strategy would be focused on the central public space which would provide future occupiers with a high quality and usable space. It is envisaged as a shared communal garden and inspired by the River Wandle, providing a soft centre with planting, creating visual interest through the seasons. A playful design approach should integrate play requirements within an overarching landscape design, through the creation of 'drumlin' landforms and natural play elements, rather than to have a more traditional fenced off 'play area'. It should be well-overlooked on all sides, with good natural surveillance throughout the day from front doors and windows facing onto the space.
- 9.55 Streets linking into and within the site have been designed to prioritise cyclists and pedestrians over vehicles. A pedestrian-priority approach utilising minimal kerb heights and a unifying surface material will promote the character of a 'home zone' where resident walking is prioritised over driving.
- 9.56 The landscape design has been further developed from PRP and Committee to improve the north-west corner by including a community garden, enlarging the central spaces, reducing car-parking numbers, improvements to the south-west garden to include ecological features and fencing, additional pedestrian/cycle route to Stafford Road/Duppas Hill and enhanced pedestrian routes from Old School Place. Whilst a sufficient level of detail has been provided to satisfy officers that the landscaping will be of high quality, the final selection of materials and planting schedule (as well as boundary treatment) would be controlled and delivered through the use of a planning condition.

Biodiversity

- 9.57 Given the existing condition of the site and the fact that it has not been identified by the CLP as having biodiversity importance, the ecology report does not identify any protected species. A number of net biodiversity gains have been proposed including bird boxes located on Block D and bat boxes on Block C, as well as a wide lighting, variety of planting including a wildflower grassland and hedgehog friendly features. Further details of the full range of biodiversity works incorporating those currently proposed would be required by condition.

Sustainability

- 9.58 Policy SP6.2 requires new development to make the fullest contribution to minimising carbon dioxide emissions, which requires new dwellings to be zero carbon, meaning achieving a 35% reduction in regulated carbon emissions over Part L 2013, with the remaining regulated CO2 emissions, to 100%, to be offset through a cash in lieu contribution.
- 9.59 The scheme is expected to achieve a 28.3% reduction in regulated carbon emissions through the inclusion of roof mounted photovoltaic panels. The remaining regulated

CO2 emissions shortfall would be covered by a carbon offset payment (of 214,920) which would be secured through the S.106 agreement and conditions.



Image 10: Proposed landscape plan

Transport, Parking and Highways

Trip Generation and Impact on Surrounding Transport Network

- 9.60 In order to assess the impact on the existing surrounding transport network a Transport Assessment was submitted. The site has a public transport accessibility level (PTAL) of between 1a and 3 across the site, which represents poor to moderate level of access to public transport services.
- 9.61 In terms of trip generation, the development is forecasted to have 76 and 59 pedestrian trips during the AM and PM peak periods respectively. In terms of vehicular traffic impacts, the estimated generated vehicular trips are 16 and 22 trips during the AM and PM peak periods respectively. This would equate to 1 car every 4 minutes for AM and 1 car every 3 minutes for the PM. The forecasted additional vehicular movements would result in an immaterial impact on the existing local highway network.
- 9.62 The results from the parking stress survey confirm that the Old School Place, Layton and Page Crescents experience relatively high levels parking stress and in general, parking stress varies between streets in the area. The survey also estimated the average total parking stress for the area at 53%. The application site is currently not included within a Controlled Parking Zone although it would be prudent to further manage car parking within the area – should the Western CPZ be expanded in the future by restricting parking permits for future occupiers of this development, to be secured through the S.106 Agreement.

- 9.63 A framework travel plan has been submitted which details some of the initiatives in order to ensure that sustainable methods of transport are promoted (such as marketing initiatives etc.). A full travel plan will be secured via the S.106 Agreement which will be need to be appropriately monitored.
- 9.64 In order to demonstrate that the impacts upon the local highway network during the construction period are acceptable, the applicant has submitted a draft Construction Logistics Plan (CLP) which proposes measures including trained traffic marshals, co-ordinated site deliveries and the use of pre-agreed routes for deliveries. Both TfL and your officers support the measures proposed, full details of which would be required by condition. The Council would seek to ensure that no deliveries take place between 07.30 and 9.30 and 16.00 and 18.30 which is in line with the approach for other development sites. The existing nursery (Busy Bees) have provided a letter which states that they have been in continual dialogue with the applicant and the proposal would not impact on the operation of the nursery.
- 9.65 The development would lead to changes to the existing and introduction of new vehicle crossovers. The applicant proposes to provide a vehicular access to the development from Old School Place together with additional pedestrian links to West Duppas Hill Path (to the east) and Stafford Road/Duppas Hill Road (to the North). Officers have sought to enhance pedestrian links from Old School Place and through the development to Stafford Road, to enhance the pedestrian/cycle time to Waddon Station. This had been investigated by the applicant and due to land ownership this has not been possible at this time. The applicants have proposed the link from the development to Stafford Road/ Duppas Hill Road (alongside the western boundary of the site towards the northern end of the site controlled by the applicant). This would be subjected to a planning condition - including safety and security measures. Through the legal agreement, the route would be further considered as part of the school development stage. Whilst the pedestrian routes lack natural surveillance and would not be particularly attractive, CCTV is also proposed across the site and along the pedestrian routes into the site and through the park.
- 9.66 The residential vehicular access would be created across the land on the existing nursery car park. Two vehicular access points, namely Stafford Road and the Old School Place are safeguarded for the school through the legal agreement together with any necessary S.278 works. A contribution towards yellow line waiting restrictions/alternative road markings on roads within the vicinity of the site are necessary to ensure that vehicle movements to and from the development can take place efficiently and safely.

Deliveries and Servicing

- 9.67 The development is anticipated to generate approximately 17 service vehicle trips per day (including waste collections, online shopping deliveries etc.). Given the lightly trafficked nature of the development, it is proposed that servicing and delivery vehicles would stop on the carriageway by the entrance to the dwellings whilst loading and unloading is taking place. Whilst a draft Delivery and Servicing Plan (DSP) has been submitted, a full DSP would be secured by condition.

Parking Provision

9.68 During the course of application the number of car parking spaces have been reduced from 101 to 90. The revised car parking provision is as follows:

- 90 spaces for 126 units, including 13 disabled bays and one visitor disabled bay. This is equivalent to 0.7 spaces per unit.
- 24 spaces for Busy Bees nursery as per the existing situation.

9.69 The reduction in the number of car parking spaces is supported by TfL and the Council's Strategic Transport Team. The proposal will provide 230 long-stay and 8 short-stay cycle parking spaces, which is acceptable.

9.70 There were previous comments from officers, Planning Committee and PRP that the submitted design appeared to be car dominated and there were requests to reduce road widths and to explore stacking. The following changes have been made to help improve the perception of the internal streets:

- On-street disabled parking bay widths have been reduced from 3.6m to 2.7m. The reduced width meets the minimum requirement set out in Department for Transport's Inclusive Mobility guidance (2005).
- On-street disabled parking removed from the one-way diagonal street by Block D to reduce the overall width of the street. These are relocated to the parking courts and the relocation increases the area available for the central park.
- Remove the existing turning head adjacent to the nursery to provide a continuous direct footway on the western side of Old School Place to the site.
- Amendments to the central park geometry means that a large vehicle would not need the full 7.5m width of the shared street by Block A to manoeuvre. The space would be reallocated as 2.0m footway and 5.5m carriageway.
- The link to Duppas Hill Park has been widened to improve access for pedestrians and cyclists.
- Road width of 5.5m is retained to allow for a servicing vehicle to stop on-street and another vehicle to pass while loading or refuse collection is taking place. Given the location and proximity of entrances, localised narrowing is not proposed to reduce the road width.

9.71 Your officers support the changes proposed and conditions are attached with regards to Electric Vehicle Charging Points, details of cycle parking and storage, blue badge and car parking spaces, visibility sight line and splays and swept paths.

9.72 Officers have requested that provision be made for car clubs. At this stage, it is not expected that car club operators would find the site to be financially viable due to the scale of the development, local car ownership levels and public transport accessibility. However, two of the proposed car parking spaces by the entrance to the development could be dedicated car club bays in the future. The car club provision, membership for future occupiers and costs to provide this, will be secured via the S.106 Agreement.

Refuse Provision

9.73 Refuse storage is located within each block of flats, adjacent to the cores. The houses would have refuse storage located within their property curtilage. Provision will be made for both general waste and recycling. Refuse storage requirements have been incorporated in accordance with advice from the Council's waste officers. Swept path analysis has been undertaken using a refuse vehicle with dimensions specified in

LBC's Waste Recycling in Planning Policy Document (Oct 2018). It is proposed that the refuse vehicle would travel one-way clockwise around the site and wait on the carriageway by the refuse stores whilst bins are unloaded. This is acceptable subject to a condition requiring details and it to be completed prior to first occupation and retained in perpetuity.

Other Planning Matters

- 9.74 An air quality assessment has been submitted given the scale of the proposed development and its location within an Air Quality Management Area (AQMA). Whilst this demonstrates the development would be suitable for the end use, to mitigate against the cumulative impacts of non-road transport sources of emissions from sites such as this, a contribution of £12,600 towards either on-site or off-site air quality improvements would be secured via the S.106 Agreement and a condition to secure details of a Construction Logistics Plan which is acceptable.
- 9.75 The site sits within Flood Risk Zone 1 (and thus is considered to be at a low risk of fluvial flooding) and parts of the site itself is at a medium/high risk of flooding from surface water. A SuDS strategy has been proposed which incorporates swales, bio-retention areas, geo-cellular tanks and permeable paving. The proposed strategy reduces the risk of surface water flooding as far as it reasonably practicable and is supported by the LLFA subject to a condition being imposed requiring the submission of evidence of correspondence from Thames Water to demonstrate their agreement to the proposed point of connection and discharge rate.
- 9.76 A contaminated land report submitted with the application concluded further intrusive reports should be undertaken. In order to ensure that any potential contaminated land on site is appropriately remediated, conditions are imposed.
- 9.77 An archaeological report submitted with the application concluded the archaeological potential is considered to be low-medium. Any impact to below-ground archaeological remains can be mitigated through an agreed programme of archaeological works, and conditions shall be imposed.
- 9.78 A health impact assessment has been submitted which has established that a population of around 711 people will be generated by the development, including 36 primary and 10 secondary school children. The report concludes that there is sufficient provision of health services within the local area to support the development, that there is sufficient capacity at both primary and secondary school level to accommodate the anticipated child yield, as well as sufficient access to social infrastructure (such as parks and community centres) and employment and training opportunities. Ensuring there are adequate employment and training opportunities for local people is secured through a contribution and clauses in the legal agreement. As such the development is therefore expected to allow its future occupants to have a healthy lifestyle and is acceptable.

Conclusions

- 9.79 All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted for the reasons set out above, subject to the completion of a legal agreement. The details of the decision are set out in the RECOMMENDATION.

Appendix 1: Planning Policies and Guidance

The following lists set out the most relevant policies and guidance, although they are not exhaustive and the provisions of the whole Development Plan apply (in addition to further material considerations).

London Plan

Policy 3.3 Increasing Housing Supply
Policy 3.4 Optimising Housing Potential
Policy 3.5 Quality and Design of Housing Developments
Policy 3.6 Children and Young People's Play and Informal Recreation Facilities
Policy 3.8 Housing Choice
Policy 3.9 Mixed and Balanced Communities
Policy 3.12 Negotiating Affordable Housing on Individual Private Residential and Mixed Use Schemes
Policy 3.13 Affordable Housing Thresholds
Policy 4.12 Improving Opportunities for All
Policy 5.2 Minimising Carbon Dioxide Emissions
Policy 5.3 Sustainable Design and Construction
Policy 5.4A Electricity and Gas Supply
Policy 5.7 Renewable Energy
Policy 5.9 Overheating and Cooling
Policy 5.10 Urban Greening
Policy 5.11 Green Roofs and Development Site Environs
Policy 5.12 Flood Risk Management
Policy 5.13 Sustainable Drainage
Policy 5.14 Water Quality and Wastewater Infrastructure
Policy 5.15 Water Use and Supplies
Policy 5.17 Waste Capacity
Policy 5.21 Contaminated Land
Policy 6.1 Strategic Approach
Policy 6.3 Assessing Effects of Development on Transport Capacity
Policy 6.9 Cycling
Policy 6.13 Parking
Policy 7.1 Lifetime Neighbourhoods
Policy 7.2 An Inclusive Environment
Policy 7.3 Designing Out Crime
Policy 7.4 Local Character
Policy 7.5 Public Realm
Policy 7.6 Architecture
Policy 7.8 Heritage Assets and Archaeology
Policy 7.14 Improving Air Quality
Policy 7.15 Reducing and Managing Noise, Improving and Enhancing the Acoustic Environment and Promoting Appropriate Soundscapes
Policy 7.19 Biodiversity and Access to Nature
Policy 7.21 Trees and Woodlands

The Mayor of London has published and adopted Supplementary Planning Guidance, of which the Affordable Housing and Viability SPG, Housing SPG, Play and Informal Recreation SPG and Sustainable Design and Construction SPG are of relevance.

Croydon Local Plan (CLP)

The Croydon Local Plan was adopted on the 27th February 2018 and the main relevant policies to this application are as follows:

SP2 Homes

DM1 Housing Choice for Sustainable Communities

SP3 Employment

SP4 Urban Design and Local Character

DM10 Design and Character

DM13 Refuse and Recycling

DM16 Promoting Healthy Communities

DM18 Heritage Assets and Conservation

SP5 Community Facilities

SP6 Environment and Climate Change

DM23 Development and Construction

DM24 Land Contamination

DM25 Sustainable Drainage Systems and Reducing Flood Risk

SP7 Green Grid

DM27 Protecting and Enhancing our Biodiversity

DM28 Trees

SP8 Transport and Communication

DM29 Promoting Sustainable Travel and Reducing Congestion

DM30 Car and Cycle Parking in New Development

DM38 Croydon Opportunity Area

Appendix 2: Drawing Nos

Site Plans

432-dRMM-ZZ-00-DR-SIT-A-1000 Site Location Plan P02

432-dRMM-ZZ-00-DR-SIT-A-1001 Proposed Site Plan P04

GA Plans

432-dRMM-ZZ-00-PLA-A-2000 GA Plan - Level 00 P02
432-dRMM-ZZ-01-PLA-A-2001 GA Plan - Level 01 P02
432-dRMM-ZZ-02-PLA-A-2002 GA Plan - Level 02 P02
432-dRMM-ZZ-03-PLA-A-2003 GA Plan - Level 03 P02
432-dRMM-ZZ-04-PLA-A-2004 GA Plan - Level 04 P02
432-dRMM-ZZ-RF-PLA-A-2005 GA Plan - Level 01 P02

Unit Types

432-dRMM-ZZ-02-PLA-A-2600 Detail House Type Plans - Block A
Apartments - Level 2 P01
432-dRMM-ZZ-XX-PLA-A-2601 Detail House Type Plans - Block D
Maisonette - Level 0 + 1 P01
432-dRMM-ZZ-XX-PLA-A-2602 Detail House Type Plans - Block C -
Level 0, 1 + 2 P01
432-dRMM-ZZ-XX-PLA-A-2603 Detail House Type Plans - Block D -
Level 4 P01
432-dRMM-ZZ-XX-PLA-A-2604 Detail House Type Plans - Block A –
Wheelchair Flats Level 04 P01

Sections

432-dRMM-ZZ-XX-SEC-A-3000 GA Sections - Block A - section A-A
+ block D - Section B-B P01

GA Elevations

432-dRMM-A-XX-ELE-A-4000 GA Elevations - Block A - South and
North Elevations P01
432-dRMM-A-XX-ELE-A-4001 GA elevations - block A - West, East
and Sectional Elevations P01
432-dRMM-C-XX-ELE-A-4002 GA Elevations - Block C - East,
North, West and South Elevations P01
432-dRMM-D-XX-ELE-A-4003 GA Elevations - Block D - South and
North Elevations P01
432-dRMM-D-XX-ELE-A-4004 GA elevations - Block D - West, East
and Sectional Elevations P01

Bay Elevations

432-dRMM-A-XX-ELE-A-4500 Bay Elevations - Block A - South and
North Elevations P01
432-dRMM-C-XX-ELE-A-4501 Bay Elevations - block C - East and
West Elevations P01
432-dRMM-D-XX-ELE-A-4502 Bay Elevations - Block D - South and North Elevations
P01

Landscaping

6696-L-GA-00-110 General Arrangement: Landscape (Ground Floor Architectural
Plan) D

Appendix 3: BRE Guidance Terms

Daylight to existing buildings

The BRE Guidelines stipulate that the diffuse daylighting of the existing building may be adversely affected if either:

- the vertical sky component (VSC) measured at the centre of an existing main window is less than 27%, and less than 0.8 times its former value (or reduced by more than 20%), known as “the VSC test” or
- the area of the working plane in a room which can receive direct skylight is reduced to less than 0.8 times its former value known as the “daylight distribution” test.

Sunlight to existing buildings

The BRE Guidelines stipulate that the sunlight of an existing window may be adversely affected if the centre of the window:

- receives less than 25% of annual probable sunlight hours (APSH), or less than 5% of annual winter probable sunlight hours between 21 September and 21 March (WPSH); and
- receives less than 0.8 times its former sunlight hours (or a 20% reduction) during either period; and
- has a reduction in sunlight received over the whole year greater than 4% of annual probable sunlight hours.

If one of the above tests is met, the dwelling is not considered to be adversely affected.

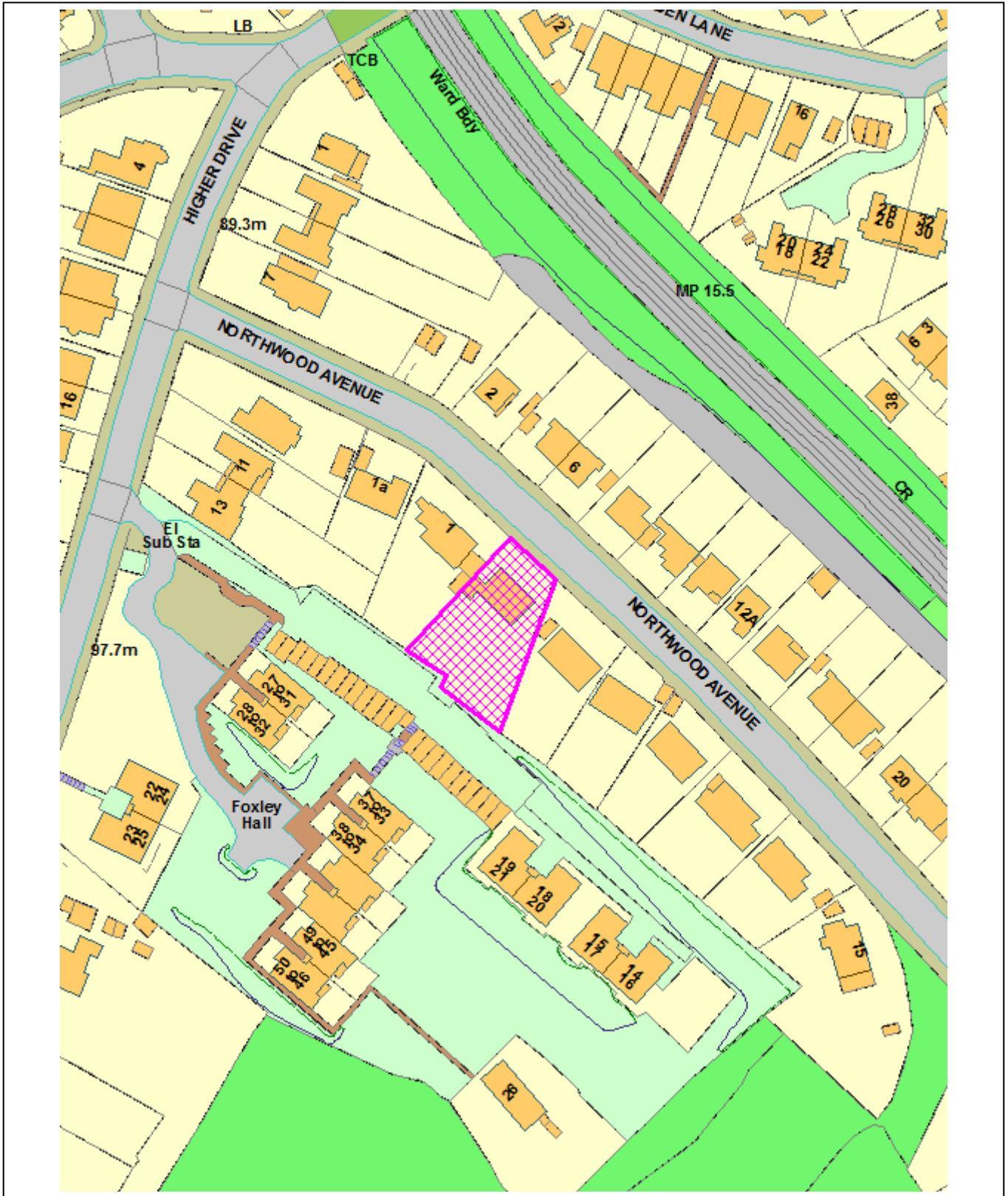
Daylight to new buildings: Average Daylight Factor (ADF)

The ADF test calculates the average illuminance within a room as a proportion of the illuminance available to an unobstructed point outdoors, under a sky of known illuminance and luminance distribution.

The BRE Guidelines stipulate that kitchens should attain at least 2% ADF, living and dining rooms at least 1.5% ADF and bedrooms at least 1% ADF.

Sunlight to gardens and outdoor spaces

The BRE guidelines look at the proportion of an amenity area that received at least 2 hours of sun on 21st March. For amenity to be considered well sunlight through the year, it stipulates that at least 50% of the space should enjoy these 2 hours of direct sunlight on 21st March.



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PART 6: Planning Applications for Decision

Item 6.2

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/02532/FUL
 Location: 3 Northwood Avenue, Purley, CR8 2ER
 Ward: Purley and Woodcote
 Description: Demolition of a single family dwelling and erection of one 3-storey block, containing 2 x 3 bedroom, 3 x 2 bedroom and 2 x 1 bedroom units with associated landscaping, 1 parking space, cycle storage and refuse store.
 Drawing Nos: 3923; 3923/1; 3923/2; 3923/4; 3923/5; 3923/6; 3923/7; 3923/8; 3923/8; 3923/9; 3923/9 Rev A; 3923/10; 3923/12; UA/TP1
 Applicant: Mr Luke Mayle
 Agent: Lee Richardson
 Case Officer: Chris Stacey

	1B2P	2B3P	3B5P	TOTAL
TOTAL	2	3	2	7

All units are for private sale

Car parking spaces	Cycle parking spaces
1	15

1.1 This application is being reported to committee because objections above the threshold in the Committee Consideration Criteria have been received.

2.0 RECOMMENDATION

- 2.1 That the Planning Committee resolve to GRANT planning permission.
- 2.2 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

- 1. Time limit of 3 years
- 2. Implemented in accordance with approved drawings
- 3. Submission of a construction management plan
- 4. Further details of sustainable drainage measures
- 5. Materials and detailed drawings to be submitted
- 6. Landscaping details to be submitted (including child play space, boundary treatments, external lighting and maintenance measures)
- 7. Details of cycle parking and cycle ramp
- 8. Details of ecological enhancement measures
- 9. Provision of electric vehicle charging point
- 10. Refuse store to be provided prior to first occupation

11. Obscure glazing and non-openable windows below 1.7m (where necessary)
12. 19% carbon reduction and 110litre water usage
13. In accordance with details and recommendations of arboricultural report
14. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

1. Community Infrastructure Levy
2. Code of practice for construction sites
3. Wildlife protection
4. Refuse collection arrangements
5. Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

3.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of intensifying the residential use of the existing site to provide a greater quantum of homes than existing is acceptable.
- The design and appearance of the development is acceptable.
- The living standards for future occupiers would be acceptable and Nationally Described Space Standard (NDSS) compliant.
- The living conditions of adjoining occupiers would be protected from undue harm subject to conditions.
- The level of parking and impact upon the local transport network is considered acceptable.
- The proposal's impact on trees and biodiversity is acceptable subject to conditions. Sustainability aspects can be controlled by conditions.
- The proposed flooding and sustainable drainage measures are acceptable subject to conditions.

4.0 SITE LOCATION AND PROPOSAL DETAILS

Site and Surroundings

- 4.1 The application site varies in width from 15m to 25m and is 35m in depth, has an overall area of 0.07ha, and is currently occupied by a detached bungalow with accommodation in the roof. The site features a tiered garden to its front alongside a sloped driveway leading to an attached garage which is accessed via a dropped kerb. A large garden sits to the rear of the existing property and features a notable rise in land level towards its rear, with the end of the rear garden sitting circa 7m higher than the front of the site.



Figure 1: Location of site

- 4.2 The site sits on the south-west side of Northwood Avenue, a two-way residential side street which terminates just to the north of the site at its junction with Higher Drive, and sits approximately 600m south-east of the boundary of Purley District Centre. The surrounding area is principally residential in character featuring a variety of detached and semi-detached properties, with some modest sized flatted blocks located to the rear of the site on Foxley Hall. The site does not fall within a conservation area and does not sit in close proximity to any statutory listed buildings. Whilst the site falls outside of the Purley CPZ, it does sit within a ‘free bay area’ with parking restrictions applicable outside of the demarcated bays.

Proposal

- 4.3 The application seeks to demolish the existing property and erect a 3 storey (inclusive of ground level) building accommodating 7 homes.
- The proposal would accommodate 2 x 1 bedroom, 3 x 2 bedroom and 2 x 3 bedroom apartments.
 - 1 parking space is proposed to the front of the site, accessed via the existing crossover. 15 cycle parking spaces are located to the rear of the site, accessed via a dedicated pathway to the side of the proposed building.
 - A communal garden with child play space is proposed to the rear of the site.
 - Refuse storage is located at the front of the site and has been integrated into the proposed landscaping.

Planning History

- 4.4 The following planning decisions are relevant to the application:

2 Northwood Avenue and R/O 5 Higher Drive:

18/04200/FUL – Demolition of existing dwelling house at 2 Northwood Avenue and construction of new apartment building containing 9 residential flats at 2 Northwood Avenue and the rear garden of 5 Higher Drive, and other associated works (revised proposal).

Permission granted December 2018

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

6.1 A total of 10 neighbouring properties were notified about the application and invited to comment. The number of representations received from neighbours, local groups etc. in response to notification and publicity of the application were as follows:

No of individual responses: 20 Objecting: 19 Supporting: 1

No of petitions received: 0

6.2 The following issues were raised in representations that are material to the determination of the application, and they are addressed in substance in the next section of this report:

SUMMARY OF OBJECTIONS	RESPONSE
Principle of development	
Overdevelopment	The development is suitable for its site and has no unacceptable impact on neighbours, character or highway safety
No need for additional housing here given the Council is currently meeting its housing targets	There is currently a 'housing crisis' and an acute need for additional housing. The intensification of existing residential sites (such as this) to provide a greater quantum of housing is in principle supported by planning policy
Design	
Out of character with surrounding area	Please refer to paragraphs 8.5 – 8.13 of this report
Poor design	Please refer to paragraphs 8.5 – 8.13 of this report
Height and massing is unacceptable	Please refer to paragraphs 8.8 – 8.9 of this report
The proposal does not provide disabled access	Please refer to paragraph 8.18 of this report
Lack of green amenity space	Please refer to paragraphs 8.16 – 8.17 of this report
Amenity	
Daylight and sunlight impacts	Please refer to paragraphs 8.19 – 8.25 of this report
Overlooking	Please refer to paragraphs 8.19 – 8.25 of this report

Increase in noise levels	The proposal is for a C3 (residential) use and would therefore not introduce noise levels which would be incompatible with other C3 (residential) uses
Disturbance from construction	A condition is recommended requiring the submission of a construction management plan prior to the commencement of works
Highways & Parking	
Insufficient car parking	Please refer to paragraphs 8.26 – 8.31 of this report
Parking survey is inadequate	The submitted parking survey has been undertaken in accordance with the 'Lambeth Methodology'
Negative impact on highway safety and local environment due to increased traffic levels	Please refer to paragraphs 8.26 – 8.31 of this report
The cycle store will not be used	Further details of the cycle store to ensure it is well designed (and thus well used) will be secured via condition
Other matters	
Detrimental impact on local wildlife	Please refer to paragraph 8.36 of this report
The proposals will contribute to global warming and carbon emissions	A condition requiring the development to be designed in a sustainable manner in order to assist in reducing carbon emissions has been proposed
Increased impacts on flooding	Measures to ensure that the proposed development does not adversely impact on flooding have been proposed, with full details of such measures being secured via condition
Detrimental impact on trees	Please refer to paragraphs 8.33 – 8.34 of this report
SUMMARY OF SUPPORT	RESPONSE
There is a lack of flats in Purley and a greater quantum of flats are required as spacious houses on large plots are not affordable	N/A

6.3 Councillor Brew made the following representations:

- Pre-application advice concerning this scheme is not available to view *[OFFICER COMMENT: This has since been added, however its contents are not material to the determination of this application, with the submission in its current form deemed acceptable.]*
- The submitted design and access statement contains a number of factual errors
- The parking stress survey methodology used is inappropriate for this location given the presence of daytime commuter parking
- The removal of two on street parking bays is unacceptable
- The proposal will result in on-street parking 'overspill'

- Overdevelopment and breaching of 45 degree line
- The proposal does not provide disabled access and should include a lift

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise.

National Guidance

7.2 Government guidance is contained in the National Planning Policy Framework (**NPPF**) revised in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up to date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Delivering a sufficient supply of homes;
- Promoting healthy and safe communities;
- Promoting sustainable transport;
- Making effective use of land;
- Achieving well-designed places.

Development Plan

7.3 The Development Plan comprises the Croydon Local Plan 2018 (**CLP**), the London Plan 2016 (**LP**), and the South London Waste Plan 2012 (**SLWP**).

7.4 A replacement for the LP (2016), in the form of the Draft London Plan 2019 (**DLP**) has been subject to public consultation and an examination in public (EiP). Whilst the LP (2016) still forms part of the adopted Development Plan, as the DLP (2019) moves further through the process to adoption it gains more weight. At present members should be advised that the DLP (2019) only carries limited weight.

7.5 Of particular relevance to this proposal is the Council's Suburban Design Guide 2019 (**SDG**) which is a supplementary planning document (SPD) intended to provide supplementary guidance to the CLP (2018).

7.6 A full list of the policies and guidance notes relevant to this application are attached in Appendix 1.

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the committee must consider are:

1. Principle of Development
2. Design and Appearance
3. Housing Quality
4. Impact on Surrounding Neighbours
5. Highways, Parking and Refuse
6. Trees, Landscaping, Biodiversity and Sustainability

7. Flood Risk and Sustainable Drainage

Principle of Development

- 8.2 The site's existing use is residential and as such the proposed redevelopment of the site for residential purposes is acceptable. Policy SP2.2 of the CLP states that the Council will seek to deliver a minimum of 32,890 homes between 2016 and 2036, with 10,060 of said homes being delivered across the borough on windfall sites (i.e. non allocated sites outside of the Croydon Opportunity Area – such as this application site). Given the above the principle of intensifying the residential use of the existing site to provide a greater quantum of homes than existing is acceptable.
- 8.3 With respect to density, the site's 'setting' can be classed as 'urban' (despite its built form being of a suburban character) due to its close proximity to Purley district centre, and given its PTAL of 3 a suitable density level range is between 200-450 habitable rooms per hectare (hr/ha) in accordance with Table 3.2 of the LP. The proposal has a density of 294 hr/ha which is within the recommended range and is thus acceptable.
- 8.4 The existing property which is to be demolished is a 3 bedroom property (as originally built) measuring 142sqm and can be classified as a family home. Policy DM1.2 of the CLP seeks to protect family homes through not allowing the net loss of 3 bedroom homes (as originally built) and Policy SP2.7 of the CLP sets a strategic target for 30% of all new homes over the plan period to have 3 or more bedrooms (where demand is greatest) which all developments are expected to contribute towards. In this instance the proposed development would provide 2 x 3 bedroom homes (representing 29% of the homes proposed and a net uplift of 1 x 3 bedroom homes on this site) which is acceptable.

Design and Appearance

Layout

- 8.5 The proposed layout of the development includes the provision of a flatted block in a similar location to the existing property (albeit with a larger footprint). A single car parking space is provided at the front of the site in the same location as the existing driveway alongside landscaping, whilst a communal garden and child play area is located at the rear of the site.
- 8.6 The front building line sits in the same location as the existing building and aligns with both of the neighbouring properties and would thus be compliant with the guidance regarding building lines set out in the CLP and SDG. The rear line of the main portion of the building would extend 5.5m beyond the rear building line of both the existing and neighbouring properties, with an inset rear projecting element extending a further 3.7m beyond this. 45 degree lines have been taken from the nearest habitable room windows of the adjacent properties, and whilst the projecting portion of the building breaches both of these lines, said breaches are very minor in nature (as illustrated by Figure 2). A gap of 1.5m and 1.8m between each side of the building and the respective boundaries to 1 and 5 Northwood Avenue has been proposed according with the relevant guidance regarding relationships to neighbouring boundaries in the SDG. Access to the building is via a communal entrance located in the centre of the front façade which provides direct access to the street, and a direct access route from the building to both the communal garden and child play area is located to the rear. An access path which follows the existing ground level on the north-west side of the

building adjacent to the boundary with 1 Northwood Avenue provides direct access from the street to the cycle store at the rear of the site.



Figure 2: Proposed site plan

8.7 The front of the site features a small area of hardstanding accommodating the proposed car parking space in the same location as the existing car parking space and another small area of hardstanding set into the slope on the opposite side provides space for the proposed refuse storage. Soft landscaping, including the provision of new trees, is provided between the car parking space, the centrally located stepped access to the communal entrance, and the refuse storage area. To the rear of the site, generous terraces to each of the ground floor apartments are provided, framed by a terraced retaining wall incorporating soft landscaping, behind which sits an area of soft landscaping which houses the communal amenity and child play space provision for the site, along with a secure cycle store housing 15 cycle parking spaces.

Scale, Height and Massing

8.8 Policy DM10.1 of the CLP requires proposals to respect the scale, height and massing of the surrounding area, whilst seeking to achieve a minimum height of 3 storeys. Section 2.10 of the SDG provides further guidance for suburban residential

developments (such as this proposal) stating that where surrounding buildings are predominantly single storey, new development should seek to accommodate a third storey within the roof space. Furthermore the SDG also states that developments do not necessarily need to step down in height towards neighbouring buildings of a lesser height.



Figure 3: Extract from the SDG



Figure 4: Proposed street scene

- 8.9 The proposed building would feature 3 storeys including the ground level and a level of accommodation within the roof, with the ridge height of the proposed building sitting 2.5m higher than 1 Northwood Avenue and 3.4m higher than 5 Northwood Avenue. Given that ridge heights along Northwood Avenue are not consistent, with variations between the scale of neighbouring properties in the local context not being uncommon, the extent of differentiation in height proposed is not considered to be of an extent that would result in a negative impact upon local character and is consistent with the guidance on building heights contained within the SDG. In light of the above it is considered that the scale, height and massing of the proposed building strikes an appropriate balance between respecting the local character whilst allowing a degree of evolution through intensifying and optimising the development potential of the site which is acceptable.

Appearance and Materials

- 8.10 The proposed architectural approach for the development follows the contemporary reinterpretation approach (as defined in the SDG), through respecting and referencing the surrounding character of the area, in terms of the building's form and material palette, whilst at the same time introducing aspects of contemporary detailing to ensure that the proposal is not simply a pastiche of surrounding buildings. Such an approach to the appearance of the proposed development is considered appropriate in this instance and would complement and respect the character of the surrounding area.



Figure 5: Proposed CGI

- 8.11 The proposed roof form of the building would be pitched, featuring two gables to the front elevation which align with the projecting elements of the front elevation below, with hips on either side, and a dormer to the rear (features found within the local area). The respective features of the roof form are well proportioned against the rest of the proposal, are not overcomplicated and are well considered, and accord with the guidance on roof forms contained within the SDG.
- 8.12 The proposed material palette consists of brick, tiling, and aluminium framed windows, and features detailing including a white glazed brick band, projecting aluminium window pods and colour contrasting soffits. The proposed material palette and detailing is well considered, would complement the surrounding character, and would accord with the guidance on appearance and materials contained within the SDG. A condition requiring compliance with the proposed material palette, and requiring the submission of material samples alongside detailed drawings of reveal depths and key junctions/features (such as the projecting aluminium window pods) has been recommended. A mixture of paving slabs and brick paving/dwarf walls would form the proposed hardscaping alongside soft landscaping features, with a condition requiring compliance with the proposed hardscaping palette, and requiring the submission of details of maintenance measures and any external lighting being recommended.
- 8.13 Whilst balconies have been proposed to the front elevation, all of these have been inset into the building form in order to respect the character of the street, featuring dark grey painted steel balustrading to their fronts. To the rear the proposed balconies feature tapered brick walls rising to 1.8m in height to their sides, and also feature dark grey painted steel balustrading to their fronts. The proposed balcony design has been

treated as an integral part of the design and is in accordance with the guidance on balcony balustrading contained within the SDG.

Housing Quality

8.14 As outlined by Figure 6 below all of the proposed apartments would meet or exceed the floor space requirements of the Nationally Described Space Standards (NDSS).

	NDSS Requirement	Proposed
FLAT A (3B5P)	86sqm	94.3sqm
FLAT B (3B5P)	86sqm	94.3sqm
FLAT C (1B2P)	50sqm	52sqm
FLAT D (2B3P)	61sqm	65.4sqm
FLAT E (1B2P)	50sqm	52sqm
FLAT F (2B3P)	61sqm	61sqm
FLAT G (2B3P)	61sqm	61sqm

Figure 6: Apartment sizes

8.15 All of the proposed apartments would be dual aspect with the exception of Flat D (a 2b3p apartment) which features an extensive south-west facing frontage, and all of the proposed apartments have dedicated storage space incorporated into their layout. As such the proposed apartments would provide a high standard of accommodation for their future occupiers.

8.16 In respect of private amenity space, all of the proposed apartments would feature their own dedicated private amenity space in the form of an external balcony or terrace, with many of these spaces notably exceeding the minimum requirements set out in the CLP. Whilst the two balconies to Flats F and G marginally fall short (by 2sqm each) of the requirements for private amenity space set out in the CLP, both of these apartments, which are dual aspect, are still considered to provide a high quality standard of accommodation for their future occupiers and would still benefit from access to a communal garden to the rear of the site.

8.17 Communal amenity space and child play space is located to the rear of the site and can be directly accessed from within the building (in accordance with the guidance contained within the SDG) via a stepped footpath of an internal corridor which provides access to all of the apartments. In line with Table 6.2 of the CLP the proposed development would be required to provide a minimum of 13.4sqm of child play space, and the proposed development would provide an area dedicated to child play space in excess of this. A condition requiring the submission of further information and details in relation to the proposed communal amenity space and child play space is recommended.

8.18 With respect to accessibility the main entrance of the proposed building sits 1.7m above of street level due to existing land levels, and given the constraints of this space it has not been possible to provide non-stepped access (i.e. Building Regulations compliant ramped access or level access) to the front of the building. A ramp would dominate the frontage of the building and be of an unacceptable appearance. Given the level of the existing house and the character of the street (with properties generally set above or below the road) in this instance this is considered to be acceptable. As such it has not been possible to provide any adaptable/accessible (M4(2)) or wheelchair adaptable/accessible (M4(3)) apartments, however notwithstanding this the development would still need to comply with Part M4(1) of the Building Regulations which still requires developments to be designed with accessibility in mind. The communal garden and child play space to the rear of the site sits at a notably higher level than the property itself, however in the absence of a substantial level of excavation, given the existing topography of the site this is unavoidable. Both the LP and the DLP advise that flexibility in regard of accessibility should be applied to developments of this scale to ensure that development is deliverable, and given the substantial topographical changes across the site, officers are content that a scheme with level access across the site (in order to comply with Parts M4(2) and M4(3) of the Building Regulations) would not be reasonably achievable on this site.

Impact on Surrounding Neighbours

8.19 There are a number of properties that surround the site which include 1 Northwood Avenue to the north-west, 5 Northwood Avenue to the south-east, 6 and 8 Northwood Avenue to the north-east and Foxley Hall to the south-west as illustrated in Figure 7 below.

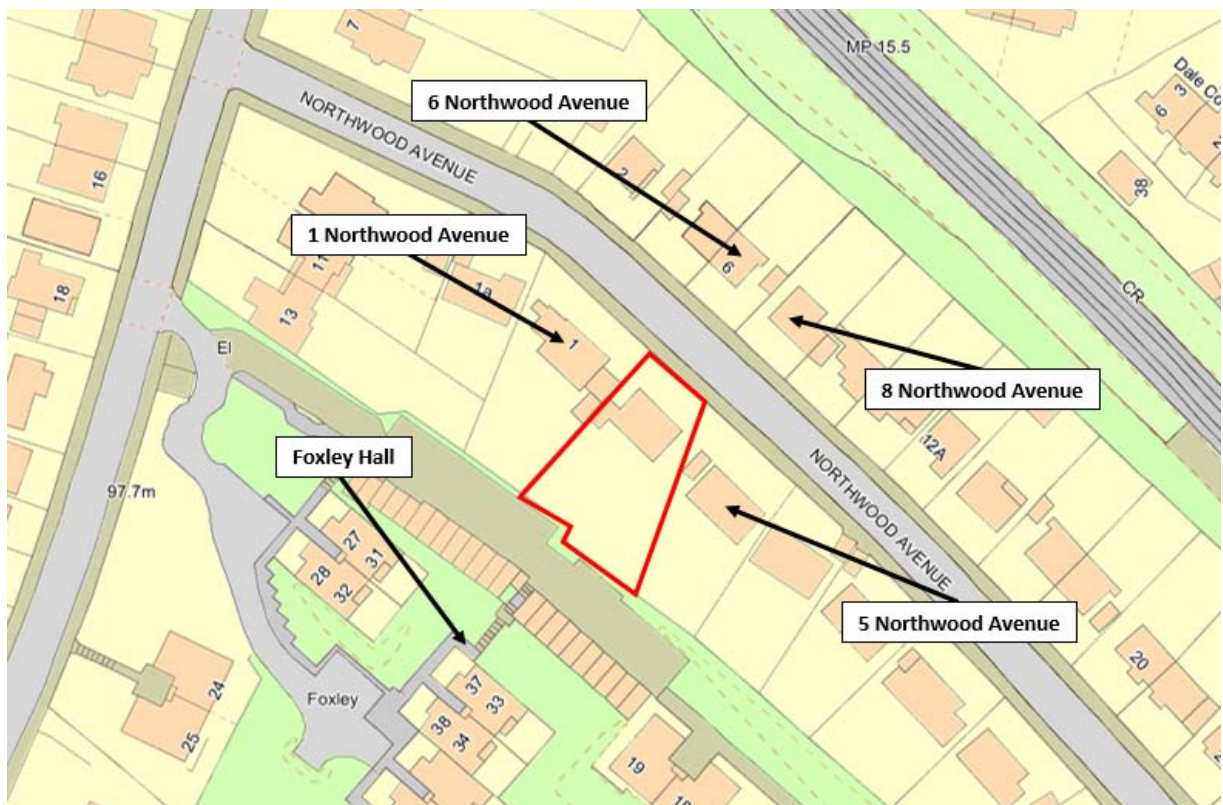


Figure 7: Surrounding neighbours

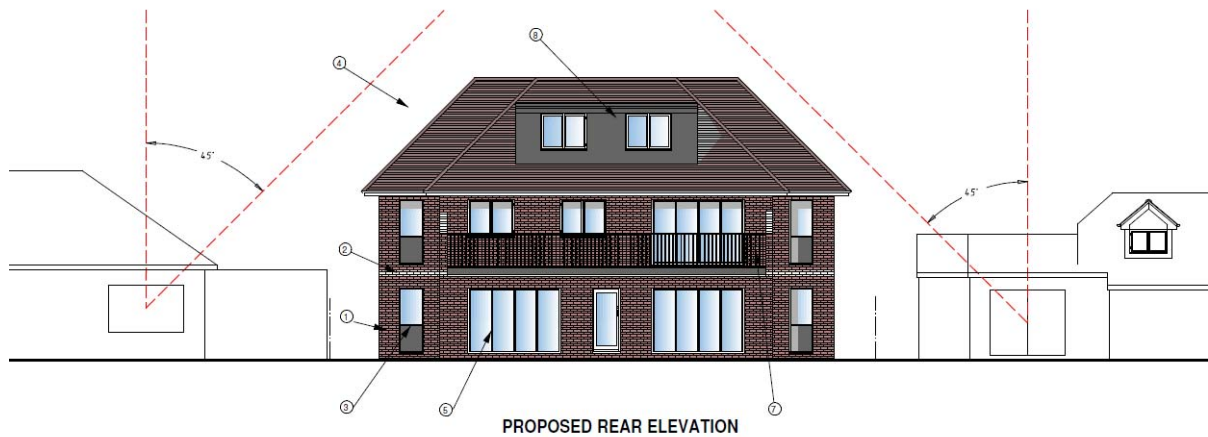


Figure 8: Rear elevation with 45 degree lines

1 Northwood Avenue

8.20 1 Northwood Avenue sits on the north-west side of the application site and is a single storey detached bungalow with an attached garage located adjacent to the boundary with the application site and does not feature any side windows on its south-east elevation (i.e. the flank elevation adjacent to the application site). The front building line of the proposed building would sit in line with the front elevation of 1 Northwood Avenue and as such would not adversely impact upon the amenity of 1 Northwood Avenue at its front. The rear building line of the projecting element of the proposed building would sit 9.2m beyond the rear elevation of 1 Northwood Avenue, and whilst a 45 degree line (taken from the centre point of the closest habitable room window on the rear elevation of 1 Northwood Avenue) shows this element of the building to slightly breach this line in plan, in elevation the proposed building sits comfortably beneath this line (see Figure 8). A daylight and sunlight assessment to understand the actual implications of the proposed development for this window has been undertaken with the results confirming that there would be no transgressions beyond those recommended by BRE guidance. As such the proposed building would not result in unacceptable amenity impacts for 1 Northwood Avenue at its rear.

8.21 The side elevation of the proposed building facing 1 Northwood Avenue features two windows (one at ground floor and one at first floor) both serving bathrooms. A condition is recommended requiring that both of these windows would need to be obscured glazed and non-openable below 1.7m (above internal floor level) in order to protect the amenity of 1 Northwood Avenue. With respect to the rear balcony at first floor level this would feature a tapered brick wall rising to 1.8m in height to its side to prevent any direct overlooking of 1 Northwood Avenue.

5 Northwood Avenue

8.22 5 Northwood Avenue sits on the south-east side of the application site and is a detached bungalow with accommodation in the roof extending to the boundary with the application site and does not feature any side windows on its north-west elevation (i.e. the flank elevation adjacent to the application site). The front building line of the proposed building would sit in line with the front elevation of 5 Northwood Avenue and as such would not adversely impact upon the amenity of 5 Northwood Avenue at its front. The rear building line of the projecting element of the proposed building would sit 9.2m beyond the rear elevation of 5 Northwood Avenue, and whilst a 45 degree line

(taken from the centre point of the closest habitable room window on the rear elevation of 5 Northwood Avenue) shows this element of the building to slightly breach this line in plan, in elevation the proposed building sits comfortably beneath this line (see Figure 8). A daylight and sunlight assessment to understand the actual implications of the proposed development for this window has been undertaken with the results confirming that there would be no transgressions beyond those recommended by BRE guidance. As such the proposed building would not result in unacceptable amenity impacts for 5 Northwood Avenue at its rear.

- 8.23 The side elevation of the proposed building facing 5 Northwood Avenue features two windows (one at ground floor and one at first floor) both serving bathrooms as well as bi-folding glazed doors to the kitchen/dining/lounge room at ground floor level. With respect to the two bathroom windows a condition is recommended requiring that both of these windows would need to be obscured glazed and non-openable below 1.7m (above internal floor level) in order to protect the amenity of 5 Northwood Avenue. In respect of the bi-folding glazed doors to the kitchen/dining/lounge room at ground floor level, said doors would sit below the land level of the 5 Northwood Avenue at this point and would thus not adversely impact on its amenity. With respect to the rear balcony at first floor level this would feature a tapered brick wall rising to 1.8m in height to its side to prevent any direct overlooking of 5 Northwood Avenue.

6 and 8 Northwood Avenue

- 8.24 These properties are located opposite the site with the separation distance between these properties and the proposal being around 24m including the presence of a road and landscaping. Given the degree of separation between the proposed development and these properties, the proposals would not result in unacceptable amenity impacts for these properties.

Foxley Hall

- 8.25 These properties are located to the rear of the site on higher ground to the application site, with the closest of these properties sitting in excess of 40m from the proposed building behind a row of garages. Furthermore there is significant established vegetation located between the application site and these properties. As such the proposals would not result in unacceptable amenity impacts for these properties.

Highways, Parking and Refuse

- 8.26 The site has a PTAL of 3 representing a moderate level of public transport accessibility, sits approximately 600m south of Purley rail station and sits on a 'hail and ride' section (without formalised bus stops) of the 434 bus route. Northwood Avenue is a non-classified residential two-way side street which terminates just to the north of the site at its junction with Higher Drive and is served by a local bus service. Whilst the site does not sit within the Purley Controlled Parking Zone (CPZ), on-street parking is managed through the provision of free unrestricted bays and single yellow lines which prevent parking between 1pm and 2pm Monday to Friday outside of the demarcated bays.
- 8.27 Vehicular access to the site is currently via a driveway and dropped kerb on the west side of the site (adjacent to 1 Northwood Avenue). The proposed development seeks to retain vehicular access to the site in the same location and also seeks to retain the same number of off-street parking spaces (one). Given the nature and level of vehicular

movement on and off of the site would remain as existing, no further amendments to the crossover are required.

- 8.28 Table 6.2 of the LP sets out maximum car parking standards for residential developments. This states that 1-2 bedroom properties should provide less than 1 space per unit, with up to 1.5 spaces per unit being provided for 3 bedroom properties. In line with the LP the proposed development could therefore provide up to a maximum of 8 spaces. It is important to note however that it is not necessarily desirable to provide car parking up to the maximum standards given the ambitions of both the LP and CLP to reduce reliance on car usage and promote/prioritise sustainable modes of transport. This position is especially relevant in this case given the site's moderate level of public transport accessibility and the fact that it is within walking distance of both Purley rail station and Purley District Centre. As such a significantly lower level of on-site car parking can be supported in this instance in the interests of reducing the development's reliance on car usage.
- 8.29 The proposed development provides a total of 1 off-street parking space (with electric vehicle charging facilities being secured via condition), having been reduced from 3 off-street parking spaces during the course of the application. Said reduction was requested by officers due to the previous proposals necessitating the removal of 2 on-street parking bays meaning that this change would not in effect alter the number of available parking spaces in the locality of the site, with the benefit that on-street parking spaces are available for all (this change also had the benefit of allowing for an increase in soft landscaping to the front of the site).
- 8.30 Given that census data projects the proposed development as giving rise to 5 cars, the proposal could potentially lead to a parking 'overspill' of 4 vehicles which usually need to be accommodated on-street in the locality. The applicant has undertaken a parking stress survey in accordance with the Lambeth Methodology, which demonstrates that existing parking stress levels within the local area are moderate (with generally 28 spaces available overnight) and thus could accommodate the anticipated 'overspill'. It is recognised that other developments have recently been granted within the vicinity of the application site (notably 2 Northwood Avenue and 3 Olden Lane), and the applicant's parking stress survey has also taken into account this 'cumulative impact' and whilst this would slightly increase the existing parking stress levels there would generally still be 20 spaces available overnight once both of these developments were fully occupied. In light of the above the proposed car parking provision and the proposal's impact upon the local highway network would be acceptable.
- 8.31 In respect of cycle parking, the proposed development seeks to provide a total of 15 cycle parking spaces, in the form of a secure cycle store located to the rear of the site adjacent to the communal amenity space. The overall quantum of cycle parking would be in excess of the minimum requirements of both the LP and CLP. In order to ensure that the cycle store is accessible a cycle ramp is proposed to be attached to the external stairs within the side pathway to ensure ease of access. A condition is recommended requiring further details of the proposed cycle storage and cycle ramp.
- 8.32 With respect to refuse storage, a dedicated refuse store located at the front of the site and sensitively integrated into the proposed landscaping is proposed, being visually screened by the topography changes of the site as well as new planting. The proposed refuse store would provide the requisite level of refuse storage for the proposed development and would thus accord with policy DM13.1 of the CLP.

Trees, Landscaping, Biodiversity and Sustainability

8.33 The existing site currently accommodates a total of 20 trees and hedges as identified by the tree survey schedule contained within the submitted Arboricultural Impact Assessment. These include 4 Category B trees and hedges and 16 Category C trees and hedges and are highlighted in Figure 9 below. None of these trees or hedges are the subject of a Tree Preservation Order (TPO).

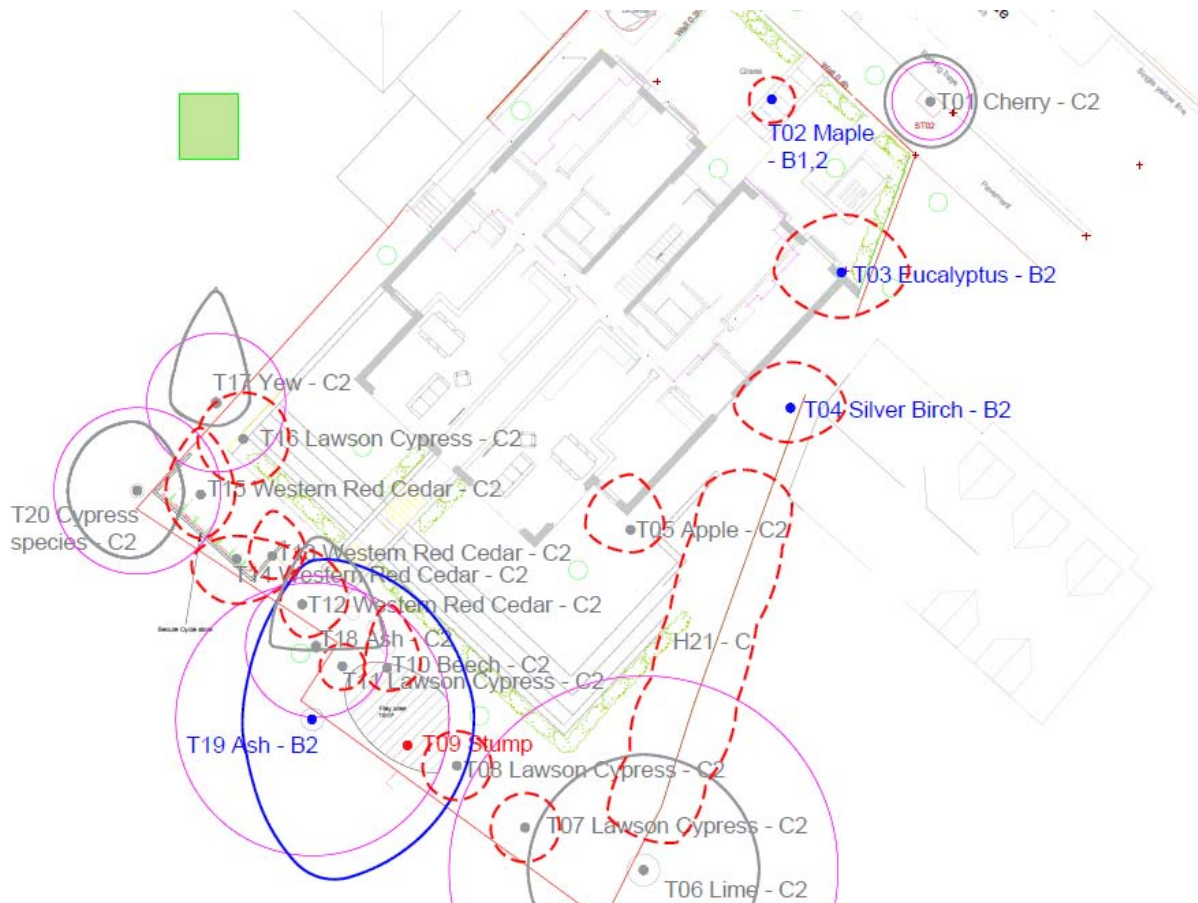


Figure 9: Existing tree and hedge survey

8.34 The proposed development seeks to remove 16 of the existing trees and hedges, 3 of which are classed as Category B trees and hedges, with the others being classed as Category C trees and hedges. Given that none of the trees or hedges that are proposed to be removed are of high quality, their removal is acceptable. In order to mitigate for the loss of the aforementioned trees and hedges and improve the landscaping quality of the site, the proposal seeks to plant 13 replacement trees and hedges, many of which will be semi-mature with an existing height of 4-4.5m and of a higher quality than those being removed. Conditions are recommended to secure the proposed replacement tree and hedge planting and ensure that the proposed development accords with the tree protection measures for the retained trees and hedges as recommended in the submitted Arboricultural Impact Assessment.

8.35 In respect of landscaping, whilst considerable portions of the site will feature soft landscaping in the form of lawns and planting, with details of the proposed planting having been submitted, hard landscaping (all of which will be permeable) in the form of paving slabs and blocks is also proposed to both the front and rear of the site. Further

details of maintenance measures and any external lighting proposed will be secured via condition.

- 8.36 A preliminary ecology report and bat survey has been submitted in support of the application to identify what habitats are present on site and look for any evidence of, or potential for, protected/notable species. This report concluded that there was no evidence of bats on site, and whilst the site contains common/widespread habitat types, none of these are of special ecological value. In order to ensure that the proposed development does not have an adverse impact upon the biodiversity value of the site and instead enhances the biodiversity value of the site in accordance with policy DM27 of the CLP, a series of ecological enhancements are proposed including new planting of value to wildlife and the provision of bat and bird boxes. A condition requiring that the development is carried out in strict accordance with the recommendations of this report is recommended.
- 8.37 In respect of sustainability conditions are recommended to ensure that the proposed development achieves a 19% reduction in CO2 emissions over the 2013 Building Regulations requirements and meets a minimum water efficiency standard of 110 litres/person/day in accordance with policy SP6.3 of the CLP.

Flood Risk and Sustainable Drainage

- 8.38 The applicant has submitted a Flood Risk Assessment in support of the application. The site is located in Flood Zone 1 and according to the Environment Agency has a very low probability of fluvial, tidal or surface water flooding. Sustainable drainage measures are proposed on the site in accordance with policy DM25 of the CLP in the form of new soakaways along with the use of permeable paving. A condition has been recommended requiring further details of the proposed sustainable drainage measures.

Conclusion

- 8.39 All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted subject to conditions and a legal agreement for the reasons set out above. The details of the decision are set in the RECOMMENDATION section of this report.

Appendix 1: Planning Policies and Guidance

The following lists set out the most relevant policies and guidance, although they are not exhaustive and the provisions of the whole Development Plan apply (in addition to further material considerations).

CLP

The Croydon Local Plan was adopted in February 2018 and the most relevant policies to this application are as follows:

- SP2 Homes
- DM1 Housing Choice for Sustainable Communities
- SP4 Urban Design and Local Character
- DM10 Design and Character
- DM13 Refuse and Recycling
- SP6 Environment and Climate Change
- DM23 Development and Construction
- DM25 Sustainable Drainage Systems and Reducing Flood Risk
- SP7 Green Grid
- DM27 Protecting and Enhancing our Biodiversity
- DM28 Trees
- SP8 Transport and Communication
- DM29 Promoting Sustainable Travel and Reducing Congestion
- DM30 Car and Cycle Parking in New Development
- DM42 Purley

The Suburban Design Guide was adopted in April 2019 as a supplementary planning document to the CLP and is of relevance.

LP

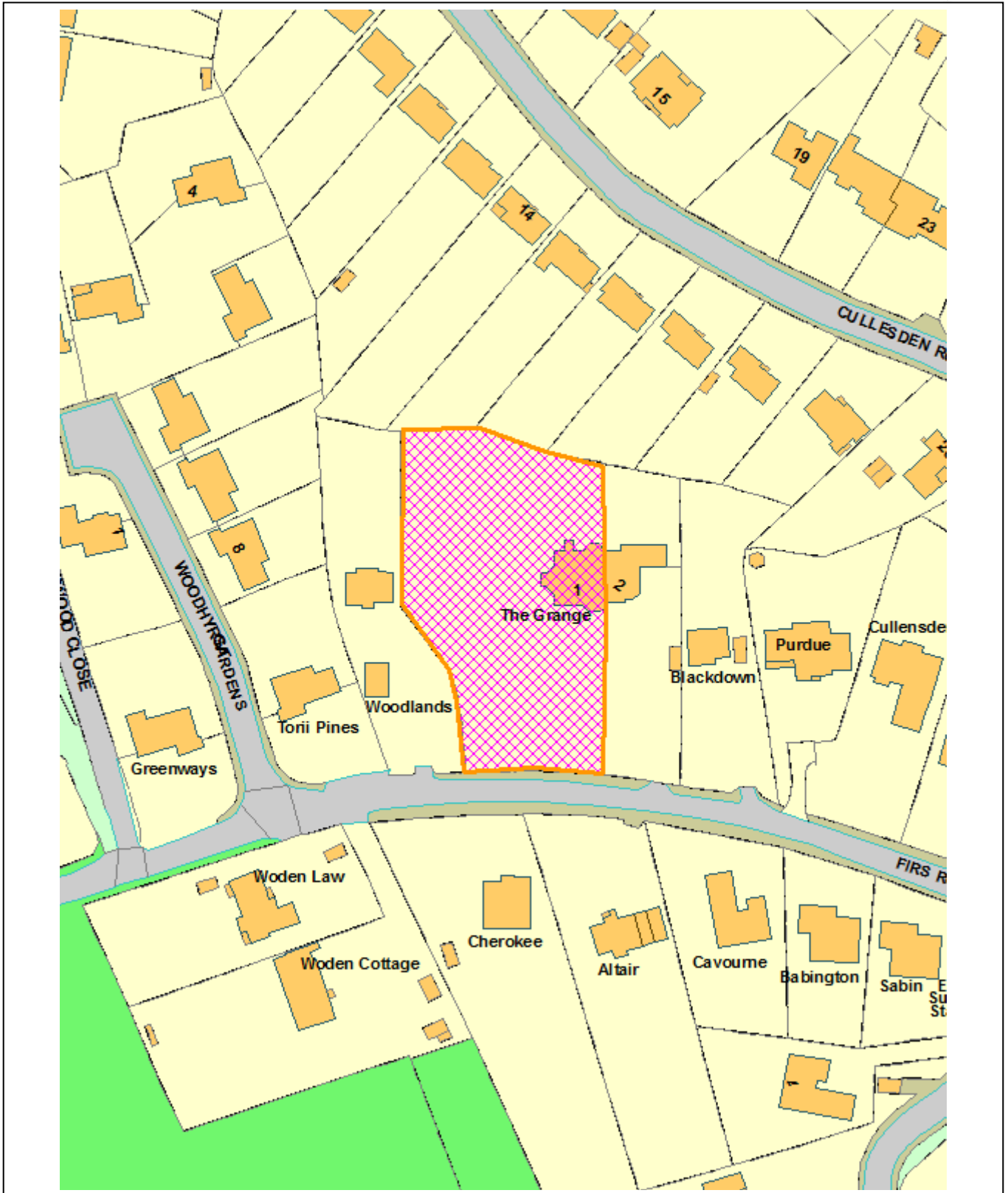
The London Plan was adopted in March 2016 and the most relevant policies to this application are as follows:

- Policy 3.3 Increasing Housing Supply
- Policy 3.4 Optimising Housing Potential
- Policy 3.5 Quality and Design of Housing Developments
- Policy 3.6 Children and Young People's Play and Informal Recreation Facilities
- Policy 3.8 Housing Choice
- Policy 5.2 Minimising Carbon Dioxide Emissions
- Policy 5.3 Sustainable Design and Construction
- Policy 5.12 Flood Risk Management
- Policy 5.13 Sustainable Drainage
- Policy 5.15 Water Use and Supplies
- Policy 5.17 Waste Capacity
- Policy 6.1 Strategic Approach
- Policy 6.3 Assessing Effects of Development on Transport Capacity
- Policy 6.9 Cycling
- Policy 6.13 Parking
- Policy 7.1 Lifetime Neighbourhoods

- Policy 7.2 An Inclusive Environment
- Policy 7.3 Designing Out Crime
- Policy 7.4 Local Character
- Policy 7.6 Architecture
- Policy 7.19 Biodiversity and Access to Nature
- Policy 7.21 Trees and Woodlands

The Housing SPG was adopted in March 2016 and the Play and Informal Recreation SPG was adopted in September 2012, both as supplementary planning guidance to the LP and are of relevance.

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Part 6: Planning Applications for Decision

Item 6.3

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/03839/FUL
 Location: 1 The Grange, Firs Road, Kenley CR8 5LH
 Ward: Kenley
 Description: Erection of a new two storey side extension and single storey rear extension and conversion of existing building to provide 9 no. 1 and 2 bedroom flats together with associated access, parking and landscaping.
 Drawing Nos: FRC/P19/01, FRC/P19/03, FRC/P19/04, FRC/P19/05, FRC/P19/06 Rev A, FRC/P19/07 Rev A, FRC/P19/08, FRC/P19/10, FRC/P19/11 Rev A, FRC/P19/12 Rev A, FRC/P19/13 Rev A,
 Applicant: Mr David Leggett
 Agent: Mr Philip Allin
 Case Officer: Sissi Yang

	1 bed	2 bed	3 bed	4 bed
Existing			1	
Proposed	2	7		

All units are proposed for private sale

Number of car parking spaces	Number of cycle parking spaces
9	18

1. This application is being reported to committee because Cllr Jan Buttinger has made a representation in accordance with the Committee Consideration Criteria and requested committee consideration. Objections above the threshold in the Committee Consideration Criteria have also been received.

2.0 RECOMMENDATION

- 2.1 That the Planning Committee resolve to GRANT planning permission
- 2.2 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. Development to be carried out in accordance with the approved drawings and reports except where specified by conditions
2. Time limit of 3 years
3. Materials including external material junctions and roof forms to be submitted.

4. Details of refuse storage as submitted
5. Landscaping scheme including boundary treatments/refuse storage to be submitted
6. All obscured glazed openings and screenings at balconies to be fully installed and maintained
7. Playspace details to be provided.
8. All flats to meet M4 (2).
9. Electronic charging points to be installed
10. Construction Logistics Plan to be submitted
11. A servicing and delivery management plan to be submitted
12. Prior to occupation the redundant pedestrian crossing will be re-instated in accordance with previously agreed details
13. Prior to occupation Parking Management Plan shall be submitted and approved
14. A Construction Management Plan will be required and approved prior to the commencement of any works on site.
15. Details of visitor cycle parking to be submitted
16. Trees - Accordance with Tree Survey.
17. Prior to commencement of any construction activity, the sub base for all new surfacing shall be installed which will be used as a haul road throughout the construction phase.
18. Ecology - Accordance with Ecological Appraisal.
19. Biodiversity Enhancement Layout to be submitted
20. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Community Infrastructure Levy
- 2) Code of practice for Construction Sites
- 3) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

- 2.3 That the Committee confirms that adequate provision has been made by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

3.0 PROPOSAL AND LOCATION DETAILS

- 3.1 The proposal includes the following:

- Erection of a new two storey side extension and single storey rear extension
- Conversion of existing building to provide 9 no. 1 and 2 bedroom flats
- Provision of private and communal external amenity space as well as children's play space

- Provision of 9 off-street spaces and associated external refuse and cycle stores (figure1)



Fig 1. Proposed Site Plan

3.2 The scheme has been amended during the application process to clarify privacy screenings at balconies and add obscured glazing at side window at ground floor level.

Site and Surroundings

3.3 The site consists of a roughly rectangular shaped plot on the northern side of Firs Road, Kenley. The application currently comprises a two storey semi-detached property which is set back from the road. The building is of a brick construction with a mock Tudor finish at first floor level. There are a number of significant trees within the front of the site along with adjacent gardens. Two trees within the front garden of the site are protected by a Tree Preservation Order (TPO) and these include a Douglas fir and Scots pine.

3.4 The application building is known as 1 The Grange, Firs Road with the other half of the semi-detached property to the east known as 2 The Grange. The neighbouring property to the west is known as Woodlands, Firs Road.

- 3.5 The surrounding area predominantly contains two storey detached dwellings set within spacious plots with the buildings set back from the road by some distance. There are a variety of building designs within the road. Firs Road has a sylvan character with well landscaped front gardens and mature trees.
- 3.6 The site is located within an area at risk of Surface Water Flooding. The site is located within a PTAL0 area.

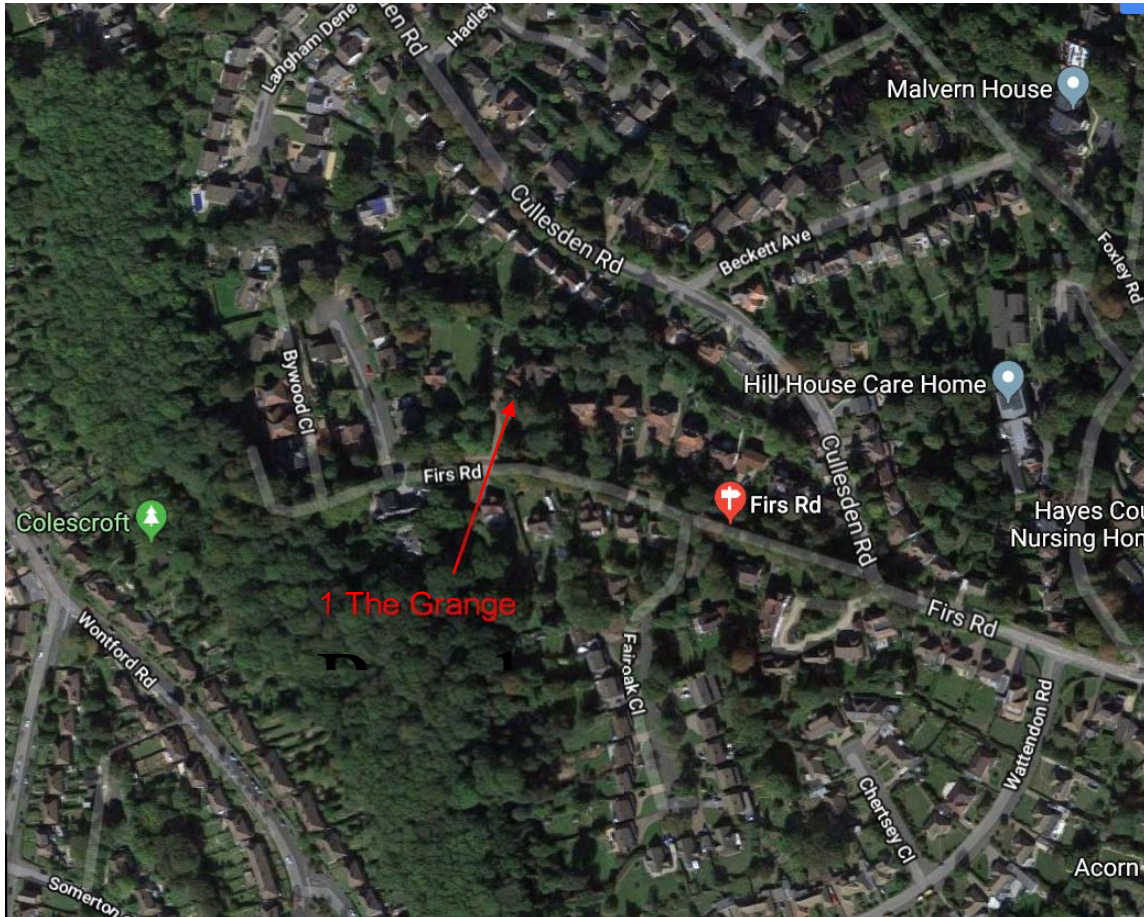


Fig 2: Aerial street view within the surrounding streetscene

Planning History

3.6 The most relevant planning history associated with the site is noted below:

- 19/02851/PRE: To erect a new extension and to convert the existing building to provide nine no.1 and 2 bedroom apartments together with associated parking.

The pre-app advised that the principle of proposed scale, design and impact on residential amenity are considered acceptable. Ecology needs to be considered at application stage.

- 19/01442/FUL: Erection of a new two storey extension and conversion of existing building to provide 8 x two bed and 1 x three bed flats (9 units in total) together with associated access, parking and landscaping. The application was refused due to reasons below.

1. The proposal, by virtue of its excessive scale and poor design, represents an unacceptable form of development that would detract from the appearance of the host building and the character of the area. The proposed development would cause significant harm to the character of the area and would be contrary to policies 7.1, 7.4 and 7.6 of the London Plan 2016, Policies SP4 and DM10 of the Croydon Local Plan 2018 and Croydon Suburban Design Guide Supplementary Planning Document 2 April 2019.
2. The proposed single storey rear extension would result in an unacceptable loss of outlook and an increased sense of enclosure to the occupiers of 2 The Grange, Firs Road due to the extensions excessive depth and proximity to the site boundary. The application is therefore contrary to Policy DM10.6 of the Croydon Local Plan 2018 and the Croydon Suburban Design Guide Supplementary Planning Document 2 April 2019.
3. Without evidence to the contrary, the proposal would result in harm to protected species and fail to provide measurable biodiversity net gain. The proposed development would harm ecological interests on site and would be contrary to Policies SP7.4 and DM27 of the Croydon Local Plan 2018.

4. SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of the development is acceptable given the residential character of the surrounding area.
- The design and appearance of the development is appropriate for its setting.
- The living conditions of adjoining occupiers would be protected from undue harm subject to conditions.
- The living standards of future occupiers are acceptable and Nationally Described Space Standard (NDSS) compliant.
- The protected trees are appropriately protected.
- The biodiversity is appropriately protected.
- The level of parking and impact upon highway safety is considered acceptable.
- Sustainability aspects can be controlled by conditions.

5.0 CONSULTATION RESPONSE

- 5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

- 6.1 The application has been publicised by 24 letters of notification to neighbouring properties in the vicinity of the application site. The number of representations received from neighbours in response to notification and publicity of the application are as follows:

No of individual responses: 48 Objecting: 48 Supporting: 0 Comment: 0

No of petitions: 1 (objecting) Signatures: 110

6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Objection	Officer comment
<i>Principle of development</i>	
Development should be family homes	This is addressed in section 8.2 to 8.4 of this report.
<i>Design and appearance</i>	
Harm to character of host property and surrounding area	This is addressed in section 8.5 to 8.13 of this report.
Not much difference to the previously refused proposal	This is addressed in section 8.5 to 8.13 of this report.
<i>Impact on amenities of neighbouring properties</i>	
Lorries will create noise and pollution and shake house foundation and tree roots	This is addressed in section 8.14 to 8.23 of this report.
Impact on amenity in terms of overlooking, outlook and light	This is addressed in section 8.14 to 8.23 of this report.
No survey is provided in respect of sunlight and daylight impact	This is addressed in section 8.14 to 8.23 of this report.
Risk assessment by structural engineer is needed for impact on adjacent walls	There is no proposed alterations to the wall.
<i>Impact of the development on the future occupiers</i>	
Water pressure is too low for 9 units	This is not a planning consideration.
Access for disabled	This is addressed in section 8.14 to 8.23 of this report.
<i>Trees and ecology</i>	

Impact on wildlife and habitat	This is addressed in sections 8.31 to 8.33 of this report.
Deer won't have access to the garden	This is addressed in sections 8.31 to 8.33 of this report.
Trees will be removed	This is addressed in sections 8.31 to 8.33 of this report.
Harmful to protected species	This is addressed in sections 8.31 to 8.33 of this report.
<i>Highways and parking</i>	
Insufficient parking provision.	This is addressed in section 8.24 to 8.29 of this report.
Significant increase in traffic is inconsistent with character of low density area and it will be detrimental to on street parking and safety	This is addressed in section 8.24 to 8.29 of this report.
Collection of rubbish/recycling bins will be difficult	This is addressed in section 8.24 to 8.29 of this report.
<i>Other material considerations</i>	
Flood risk issues	This is addressed in section 8.31 of this report.
Provision of cycle storage is doubtfully beneficial	Provision of cycle parking is required by London Plan and Croydon Local Plan
Storage for individual bins are replaced by bulky storage	This is addressed in section 8.24 to 8.29 of this report.
Sewerage issue	This is addressed in section 8.30 of this report.

Pressure on local facilities and infrastructure	This is addressed in section 8.34 of this report.
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6.3 Cllr Jan Buttinger made representations and referred the application to committee on the grounds that:

- The development is totally out of character with the area
- There will be the destruction of as many as 23 trees on this site which contradicts this Council's policy on Green Spaces
- There are considerable safety issues on this road for pedestrians as there are no footpaths
- The development is an over development of the site.

RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2016, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivering a wide choice of high quality homes;
- Requiring good design.

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency

- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture
- 7.21 Woodlands and trees

Croydon Local Plan 2018

- SP2 - Homes
- SP6.3 - Sustainable Design and Construction
- DM1 - Housing choice for sustainable communities
- SP4 – Urban Design and Local Character
- DM10 - Design and character
- DM13 - Refuse and recycling
- DM16 – Promoting healthy communities
- DM18 - Heritage assets and conservation
- SP6 – Environment and Climate Change
- DM23 - Development and construction
- DM25 – Sustainable drainage systems and reducing floor risk
- SP7 – Green Grid
- DM27 – Biodiversity
- DM28 – Trees
- SP8 – Transport and communications
- DM29 - Promoting sustainable travel and reducing congestion
- DM30 - Car and cycle parking in new development

There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016
- The Suburban Design Guide 2019

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the Planning Committee are required are as follows:

1. Principle of development
2. Townscape and visual impact
3. Housing quality for future occupiers
4. Residential amenity for neighbours
5. Access and parking
6. Sustainability and environment
7. Trees and Biodiversity
8. Other matters

Principle of Development

- 8.2 The London Plan and Croydon Local Plan identify appropriate use of land as a material consideration to ensure that opportunities for development are recognised and housing supply optimised. Approximately 30% of future housing supply may be delivered by windfall sites which provide sensitive renewal and intensification of existing residential areas and play an important role in meeting demand in the capital, helping to address overcrowding and affordability issues.
- 8.3 The site is located within an existing residential area and as such, providing that the proposal respects the character and appearance of the surrounding area and there are no other material impacts, a residential scheme such as this is in principle supported.
- 8.4 Policies aim for there to be no loss of 3 bedroom homes as originally built, homes under 130m² and that 30% of homes should be family homes (including 2 bed 4 person homes). The existing building comprises more than three bedrooms as originally built and the existing floor area is greater than 130m². The scheme proposes 9 family units (7 x 2 bedroom, 4 person units and 2 x 1bedroom, 2 person units) which exceeds 30%, so there is a net uplift in family homes.

Townscape and Visual Impact

- 8.5 The existing dwelling is not statutorily or locally listed and therefore there is no objection to its demolition.
- 8.6 The National Planning Policy Framework (NPPF) attaches great weight to the importance of design in the built environment. Policy 3.5 of the London Plan (2016) requires housing development to be of the highest quality and Policies 7.1, 7.4 and 7.6 state that development should make a positive contribution to the local character and should incorporate the highest quality materials and design appropriate to its context. Policy SP4 and DM10 of the Croydon Local Plan (2018) reinforce these policies by confirming that the Council will require development to be of a high quality and be informed by the distinctive qualities, identity, topography and opportunities of the relevant places of Croydon.
- 8.7 The proposal would include a two storey side extension to the western side and a single storey rear extension to northern side to the main building. The proposed ground floor rear extension would be to the rear of the existing building at western side with a bay window. The proposed side extension would have pitched roof and it would be set back from the front elevation of the main building and would be lower than the roof line of the original building. There would be a structure with a flat roof linking the original building and the side extension providing internal communal access for all units to front and back of the building. The design of the proposal is considered traditional with materials including bricks, roof tiles match existing, painted railings and render and glazing privacy screening.
- 8.8 Figures 3, 4, 5, 6, 7 and 8 below show a comparison between the previously refused scheme (19/01442/FUL) and the current scheme in this application.



Figure 3: Proposed Front (South) Elevation



Figure 4: Previously Refused Proposed Front (South) Elevation



Figure 5: Proposed Rear (North) Elevation



Figure 6: Previously Refused Proposed Rear (North) Elevation

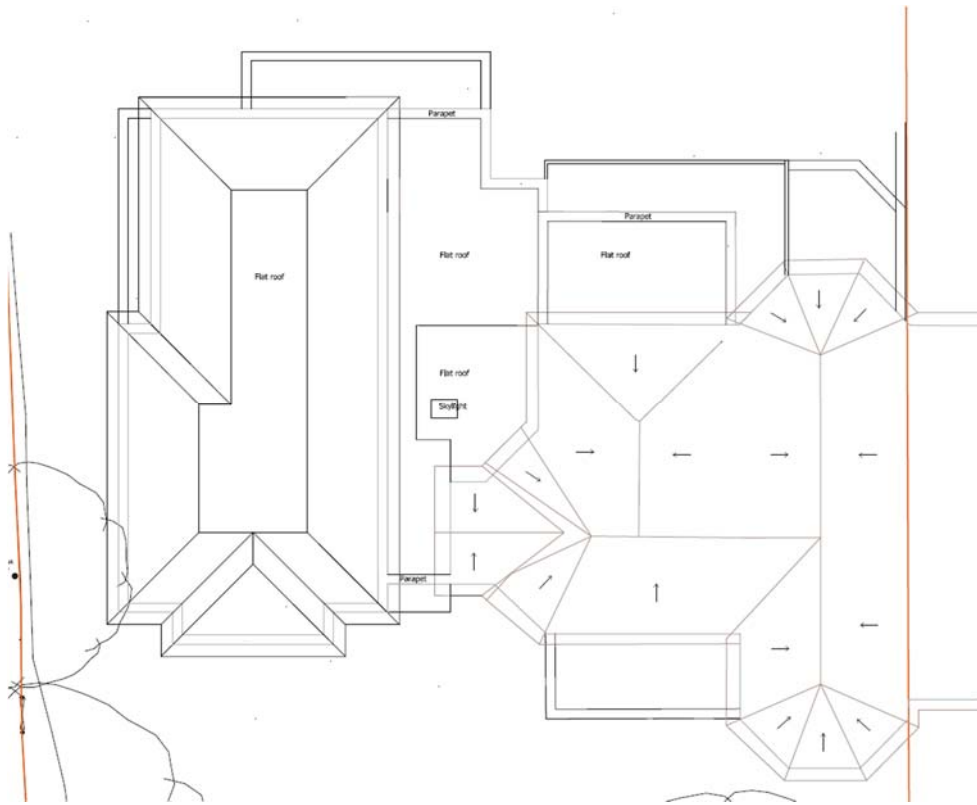


Figure 7: Proposed Roof Plan

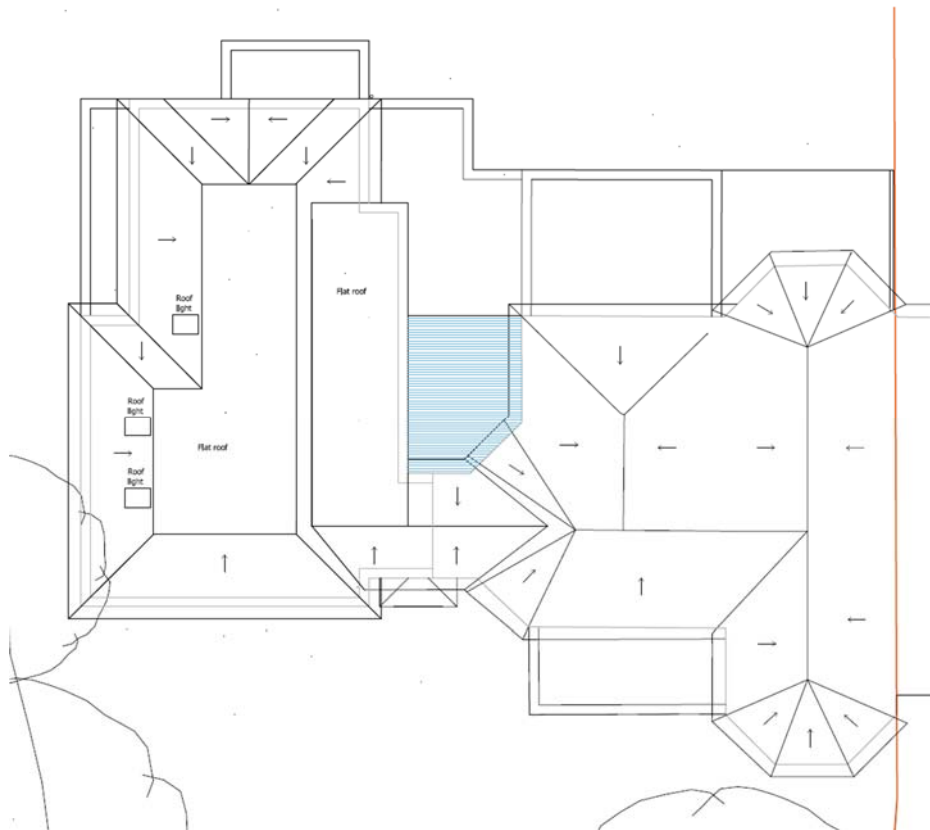


Figure 8: Previously Refused Proposed Roof Plan

- 8.9 In comparison to the previously refused scheme, the width of the extension in this application has been reduced and its footprint has been set back behind the main building line. The ridge height of the extension is now subservient to the existing building. The proposed extension now appears subservient from the street scene in terms of its scale and massing. A flat roof form to the link element is acceptable in principle. The removal of the sun room and overall simplification to the roof form is beneficial. The rear building line has been pulled back in to a depth of 3.5m and is articulated with a chamfered bay adjacent to the neighbour which is supported.
- 8.10 The extension relates well to the existing building in terms of its fenestration and detailing. The rear elevation has been amended to brick. The upright balustrade treatment will be metal and painted. The privacy screen treatment will be 1.7m high with translucent panels. The footprint has been pulled in slightly to the side to allow more space for defensible planting in front of habitable room windows which face the side access route. The proposed materials are considered acceptable. All external material junctions (including roof forms) will be controlled via a condition.
- 8.11 The proposal would have spacious gardens at front and back of the building with planting separating private and shared amenity space. Shared amenity space, children's play area and bicycle storage will be located close to the rear boundary. To the front of the building, existing access routes will be removed and most of the vegetation will be retained. Car parking and refuse/bin storage will be at the front and there will be green space and trees separating the building and proposed car parks. There will also be soft boundary treatment at the front which complements the sites verdant and sylvan setting allowing the landscaping to take prominence.
- 8.12 The proposed location of refuse stores is considered acceptable which will have 4,200 ltrs in total which would meet Council's requirements for 9 units. They need to be screened from street scene and constructed from high quality materials. Its design should be sympathetic to the main building. Further details of the refuse stores will be controlled via a condition.
- 8.13 Overall the scheme is considered to be a sensitive intensification of the site which makes the best use of the site, creates a homogenous development, which is of an appropriate scale and that respects the existing character of the wider area. It is considered that this application has overcome the issues from the previous refusal regarding design and it would be acceptable subject to conditions.

Housing Quality for Future Occupiers

- 8.14 All the units would comply with requirements set out by the Nationally Described Space Standards (NDSS) in relation to units, internal floor areas and floor to ceiling heights. All units would be afforded communal space and child play space which adhere with the CLP2018 standards. A number of the units have small side windows or windows at oblique angles to ensure that they are dual aspect. It is noted that units 6 and 9 would not have private outdoor amenity space. These are two first floor forward-facing units, one in the original building. Given that the proposal is a change of use with extensions, and that balconies cannot be easily added to the existing front elevation without appearing significantly out of place, and that it would also give rise to an unusual appearance if the extension had forward facing balconies, this is considered acceptable in principle and accords with the flexibility set out in the London Housing SPG. Each of these units has an additional 10m² floorspace over

the minimum requirements and are south-facing, so have an acceptable level of amenity.

- 8.15 Concerns were raised that no daylight and sunlight assessment was provided. Although it is not provided, the property has sufficient distance between properties to its north, south and west where most openings are located. Although units 3, 4, 8 and 9 are single aspect, units 4 and 9 are south facing and have sufficient size, private amenity space and access to a large communal area. Units 3 and 8 are north facing however they both have sufficient internal floor area, private amenity space and access to a large communal areas. Further, all of these units are constrained by being part of the existing building with minor extensions. Therefore on balance this is considered not to warrant refusal.
- 8.16 The proposed building would have internal access through to the communal area as well as externally around the building. A child play space is shown to be provided within the communal garden space (which can be secured by condition). The block plan has indicated an indicative landscaping scheme to ensure that there is a clear definition between private and communal spaces. The potential overlooking issue to windows to western side at ground are protected by the planting. A detailed landscaping scheme will be secured by condition.
- 8.17 In terms of accessibility, M4 (2) level access to front and rear communal space would be provided to all units at ground floor level which will be secured by condition. Units 1 and 2 on ground floor level would meet M4 (3) for wheelchair users which will be secured by condition.
- 8.18 Overall the development is considered to result in a high quality development including adequate family units it is provided acceptable private/communal amenities and capacity to provide child playspace which provide a good standard of accommodation for future occupiers.

Residential Amenity for Neighbours

- 8.19 The properties that have the potential to be most affected are the adjoining properties Woodlands and 2 The Grange on Firs Road. 18 & 20 Cullledon Road are located to the rear and Cherokee and Altair are located opposite on First Road. Figure 9 below indicates locations of the neighboring properties.

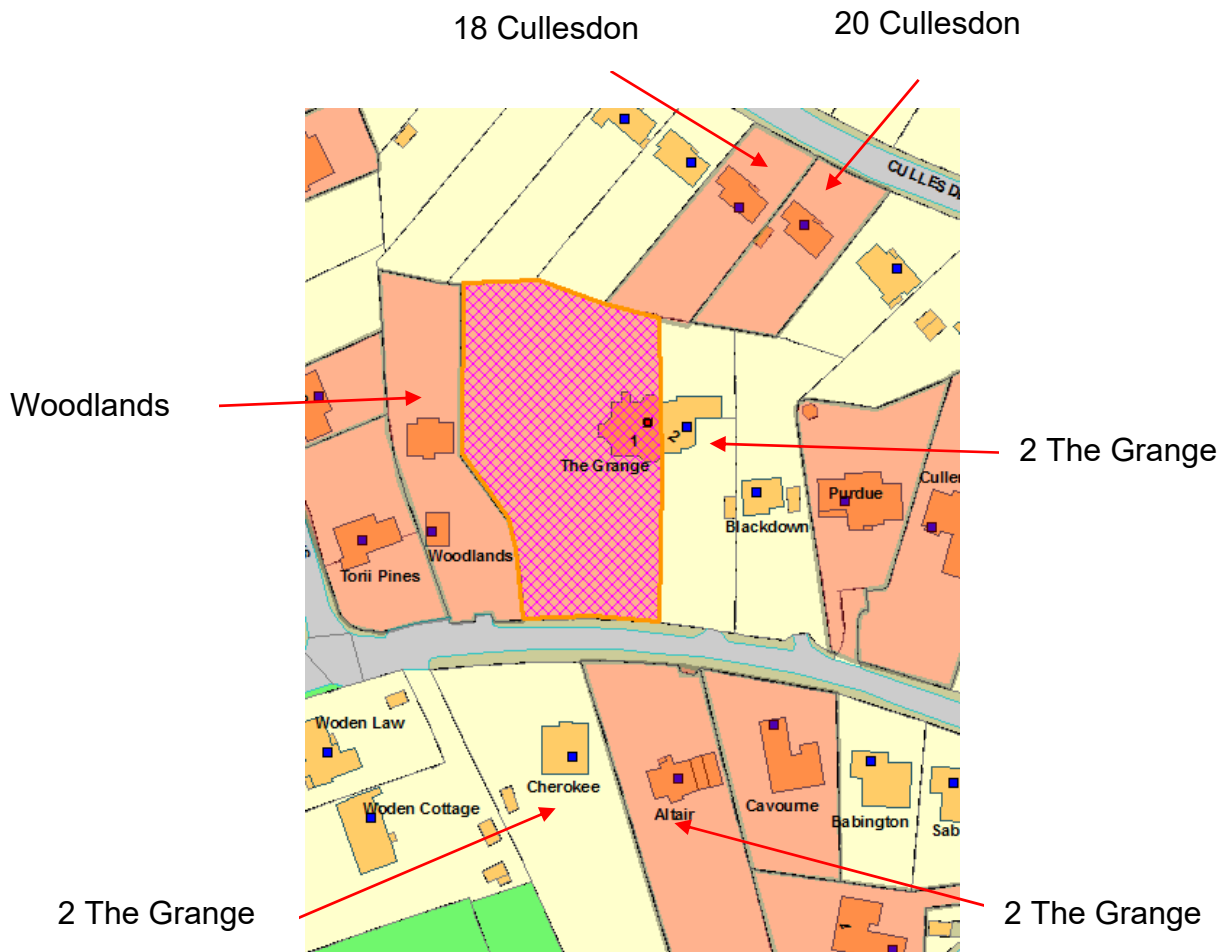


Figure 3: Existing Site Location Plan with neighbouring properties

18 and 20 Cullledon Road

- 8.20 There would be an approximate separation of 40 metres between the proposed extensions and the rear elevation of these adjoining occupiers. Owing to this significant separation between the built forms and existing mature landscaping, overall the proposal is not considered to impact upon the amenities of these adjoining occupiers.

Cherokee and Altair

- 8.21 There would be an approximate separation of 60 metres between the proposed extensions and the front elevation of these occupiers. Owing to this significant separation between the built forms and existing mature landscaping, overall the proposal is not considered to impact upon the amenities of these adjoining occupiers.

Woodlands

- 8.22 There would be an approximate separation of 20 metres between the proposed extensions and the side elevation of this property. Given its location, overall height being two storey and massing, it would not have significant impact on sunlight, daylight and outlook. The proposed side windows at first floor level would be obscured. Proposed screening of balconies would be 1.7m high and obscured glazing. This will be secured by condition. Therefore there will not be unacceptable overlooking issue to this occupier.

2 The Grange

- 8.23 One of the reasons for refusal of the previous scheme related to the impact on this property. The proposed depth of the single storey rear extension has been reduced to a maximum depth of 3.5 metres and the impact on 2 The Grange is now in line with the guidance set out in the adopted Suburban Design Guide. Further there is existing boundary treatment, thus the proposed ground floor extension would not have significantly additional impact on sunlight, daylight and outlook to this occupier. In terms of privacy, proposed screenings of balconies would be away from the boundary and would be 1.7m high and obscured glazing which will be secured by condition. Overall the proposal is not considered to detrimentally impact the amenities of this neighbouring property.

Access and Parking

- 8.24 The site has a PTAL of 0, which is the worst PTAL. The site has 2 vehicle access points to Firs Road, with one to be closed.
- 8.25 9 off street parking spaces are provided for the flats which falls within the Draft London Plan standard of up to 1.5/unit. A servicing and delivery management plan will be required in condition. This should ensure that all servicing of the site occurs from within and not Firs Road. In compliance with the London Plan, 5 electric vehicle charging points are proposed to be installed in the parking area and this can be secured by way of a condition.
- 8.26 Concerns are raised about highway safety of Firs Road. The applicant submitted visibility splays for vehicle sightlines which is considered acceptable. Sufficient cut back of planting is required to allow for the pedestrians sightlines shown on the site plan drawing. This will be secured via condition to show boundary treatment and landscaping. There will be additional traffic as the result of the proposal, however this is not considered unacceptable given its scale. A Construction Logistics Plan will be conditioned on any approval granted. Council's Transport Officer was consulted and raised no objections subject to conditions.
- 8.27 18 proposed cycle storage facilities would comply with the London Plan. The cycles would be stored in a purpose built timber structure with grass roof in the rear garden, which would be accessible externally via a cycle ramp. This is considered an appropriate approach to provide these facilities, without the need for a large structure which could potentially dominate the character and appearance of the area to be constructed at the front of the site. A visitor cycle parking space close to the entrance

to the main building must be provided. Details of the cycle parking and store will be conditioned.

- 8.28 The refuse/recycling store is located in the front of the building. Refuse/recycling storage is provided with the last receptacle at 20m from the public road and it will be within 30m from front entrance of the building which is acceptable. Details will be conditioned.
- 8.29 Taking into account the sites location within a residential area, a Construction Management Plan (CMP) will be required via condition. This condition would require a CMP to be submitted and approved prior to the commencement of any works on site.

Environment and sustainability

- 8.30 The applicant has submitted a Flood Risk Assessment (FRA) which has reviewed the existing on-site scenario and proposed a number of mitigation methods. These include the use of permeable materials and soft landscaping to reduce on and off site flood risk. The FRA has clearly stated that the mitigation methods set out (and proposed to be secured via condition) would reduce on and off-site flood risk in comparison to the existing situation. Regarding concerns raised about sewage, solutions are also proposed in the submitted FRA including flow control manhole and catchpit chamber.

Trees

- 8.31 As stated above, there are a number of significant trees within the front of the site along with adjacent garden. Two trees within the front garden of the site are protected by a Tree Preservation Order (TPO) and these include a Douglas fir and Scots pine. Car parking is provided within the root protection areas of significant trees.
- 8.32 The applicant has submitted an Arboricultural Report and Impact Assessment which confirms that 9 trees will be removed with the significant trees retained. There will be 7 trees provided new hard surface or replacement of hard surface with soft landscape. The Councils Tree Officer has confirmed that the proposal would not result in significant harm to important trees within and adjacent to the site subject to a condition requiring that the measures within the submitted report and tree protection plan are carried out in full. In addition, a landscaping scheme would be secured by planning condition. Prior to commencement of any construction activity, a temporary construction surface will be installed in a manner which does not result in harm to trees.

Biodiversity

- 8.33 One of the reasons of the previous refusal is that the proposal would result in harm to protected species and fail to provide measurable biodiversity net gain. In this application, the applicant submitted Preliminary Ecological Appraisal (March 2019) and the Phase 2 Survey Results (Bats) (May 2019) by Aspect Ecology, relating to the likely impacts of development on Designated sites, Protected and Priority species / habitats. Phase 2 Survey sets out assessment of potential effects on bats and additional requirements for mitigation further to that set out in the Ecological Appraisal (March 2019). Emergence and re-entry bat surveys did not record evidence for

roosting bats within the site. The majority of the site supports only low levels of foraging/commuting activity from common bat species. Recommendations made in this report in regards to the works associated with the residential building within the site, and the removal of trees supporting low bat roosting potential will ensure that the conservation status of local bat populations will be fully safeguarded under the scheme. Council's Ecologist is satisfied that there is sufficient ecological information available for determination of this application. Therefore it is considered that this issue has been addressed in this application. Conditions will be attached to request that the work should be carried out in accordance with Ecological Appraisal. A Biodiversity Enhancement Layout should also be submitted. Concerns regarding deer would not warrant refusal to this application.

Other matters

- 8.34 Representations have raised concerns that local services will be unable to cope with additional residents moving into the area and the impact on local infrastructure. The development will be liable for a charge under the Community Infrastructure Levy (CIL). This payment will contribute to delivering infrastructure to support the development of the area, such as local schools.

Conclusion

- 8.35 The principle of development is considered acceptable within this area. The design of the scheme is of an acceptable standard and would not harm the visual amenities of the area or adjoining occupiers. The proposed impact on the highway network and parking provision is acceptable, having taken into consideration the existing situation within the surrounding area and its location. The proposal is therefore overall considered to be in accordance with the relevant policies.
- 8.36 All other relevant policies and considerations, including equalities, have been taken into account.

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PLANNING COMMITTEE AGENDA

PART 8: Other Planning Matters

1 INTRODUCTION

- 1.1 In this part of the agenda are reports on planning matters, other than planning applications for determination by the Committee and development presentations.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 The following information and advice applies to all those reports.

2 FURTHER INFORMATION

- 2.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

3 PUBLIC SPEAKING

- 3.1 The Council's constitution only provides for public speaking rights for those applications being reported to Committee in the "Planning Applications for Decision" part of the agenda. Therefore reports on this part of the agenda do not attract public speaking rights.

4 BACKGROUND DOCUMENTS

- 4.1 For further information about the background papers used in the drafting of the reports in part 7 contact Mr P Mills (020 8760 5419).

5 RECOMMENDATION

- 5.1 The Committee to take any decisions recommended in the attached reports.

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